

GEOLOGICAL AND AVALANCHE HAZARDS  
IN SUNSHINE VALLEY

by

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## INTRODUCTION

### Scope of the Study

On July 22nd, 1977, Mr. M.G. Elston, Executive Director of Engineering, B.C. Ministry of Highways & Public Works (Highways), requested a zoning map of Sunshine Valley 'between the eastern edge of the Hope Slide and the old Sumallo Lodge site'. The zoning map is to show those areas 'which should not be developed because of rockslides, rockfalls, avalanches, etc.' but was 'not to include the effects of the Hope Slide itself'.

### Location and Limits of the Study Area

The study area is located twelve miles southeast of the municipality of Hope on Provincial Highway #3. The area as defined in Figure 1 is contained in National Topographic Series map for: HOPE 92H/6. Although the valley within the area evaluated has no formal name it is commonly referred to as Sunshine Valley. Within the context of the report that name will be utilized to define that portion of the valley from the Hope Slide in the northwest to the site of the old Sumallo Lodge at the southeast end of the area.

### Previous Work

The west half of the Hope area (NTS 92HW $\frac{1}{2}$ ) has been mapped by Monger (1970) and includes data from a report by McTaggart and Thompson (1967). These reports include a description of the lithology, stratigraphy, structure and age of the two groups of rocks which are widespread in the Cascade Mountains and are important units in Sunshine Valley. These units are stratified metasedimentary and metavolcanic rocks of the Hozameen Group, and a massive intrusive quartz diorite unit.



On January 9th, 1965, the Hope Landslide, a major catastrophic landslide, occurred at the northwest end of Sunshine Valley. Debris from this landslide is found within the area inspected. Mathews and McTaggart (1969) describe details of the landslide, those factors contributing to the slide, and present a description of the geological setting of the slide area including Sunshine Valley.

In April 1974, Golder Brawner & Associates, Vancouver, B.C. were retained by D.M. Low, Developer, to conduct a general geotechnical investigation on:

"the stability of the Sunshine Valley slopes about 12 miles southeast of Hope, British Columbia and an opinion on the likelihood of future large-scale slope failures similar to the 1965 Hope Slide occurrence."

The report abstract (dated February 1975) states that:

"while the possibility exists of a further slide from the area just north of the 1965 slide, such a failure would pose no threat to the development"

and that:

"the remainder of the valley walls appear secure from major instability."

The above mentioned report discusses three sites in Sunshine Valley which are subject to geological hazard.

Two of the sites are subject to avalanche hazard and include an avalanche track on the eastern flank of Mt. Potter and an avalanche track above "what is now called Sumallo Lodge". The report recommends (p. 4) that:

"Development should be kept away from the path of snow moving down these and other identifiable snowslide paths."

The third area indicated was judged to be subject to rolling rock hazard. The area was located in LS3, LS4, S6, R24, T4 (Figure 1) below a cliff showing extensive open joints. With respect to this area the following recommendations were presented (p. 4):

"permanent structures not be placed within 150 ft. of the toe of the slope. If this location is otherwise very desirable, most of the problem potential from this area of loose rock can be alleviated through an adequate heavy scaling program."

In addition to the above recommendations, the Golder Brawner & Associates report stated that due to rockfall hazard and the presence of open cracks at the top of the slide no permanently inhabited structures be developed on the ground deposited or seriously disturbed by the 1965 landslide.

In 1977, Mr. M.G. Elston, P.Eng., Executive Director of Engineering, Ministry of Highways & Public Works (Highways), requested Thurber Consultants Ltd. of Victoria, B.C. to provide an overview of the potential natural hazards of the Sunshine Valley subdivision development. The report was prepared by Mr. H.W. Nasmith, P.Eng. and dated August 5th, 1977.

Nasmith recommended that:

"the Approving Officer of the Ministry of Highways should not approve any development involving permanently inhabited structures on any part of the valley bottom where the ground was deposited or disturbed by the Hope Slide."

### Acknowledgements

Advice and information were given to the writer by Mr. G.E. Miller and Mr. R. Webb, Geotechnical & Materials Branch, Ministry of Highways & Public Works (Highways) Regional Office, Kamloops. The analysis of avalanche hazard in the valley was contributed by Mr. G.L. Freer, Senior Avalanche Co-ordinator, Ministry of Highways & Public Works (Highways), Victoria, B.C.

### GEOLOGY

#### Physiography

That portion of the valley contained in the study area varies in width from 2.5 to 3.0 miles. The valley bottom is relatively flat except in those areas where deposits of glacial till, slide debris or alluvial fans produce areas with greater elevation than the valley floodplain.

The valley walls are steep, with slopes commonly greater than 30 degrees from horizontal. Since the bedding of the metasediments and metavolcanics composing the valley walls dip towards the southwest, the southwest side of the valley is slightly steeper than the northeast valley wall. Peaks on either side of the valley reach elevations of approximately 6600 ft. Maximum relief from the floor of the valley to the heights of these peaks is in the order of 4000 ft.

Throughout the flat valley bottom the water level is estimated to be generally less than 15 ft. below the surface.



Three major streams flow into the valley. Ferguson Creek flows between a valley located between Mount Coulter and Mount Potter on the southwest side of the valley. This stream enters the valley south of the Hope Slide and flows through debris from the slide to wetlands at the northwest end of the study area. Approximately 4000 feet northwest of Tashme the flow of Ferguson Creek is controlled by a series of irrigation ditches and a modified channel until it joins the Sumallo River, 0.5 miles southeast of Tashme.

The Sumallo River flows from a prominent unnamed valley east of Mount Potter and enters Sunshine Valley at Tashme. From this point, it flows, at grade, southeast as a meandering river with a well developed floodplain.

Approximately 1200 ft. north of the abandoned site of Sumallo Lodge, on the northeast side of the valley, a third major stream, with headwaters directly east of Johnson peak, flows over a well developed alluvial fan to join the Sumallo River on the opposite side of Sunshine Valley.

Since the climate is moderate, tree growth is abundant on the valley flanks. Conifers including Douglas Fir are common. Cedar is rare, Poplar is found growing on well drained debris from an ancient debris flow on the northeast side of the valley between Tashme and the Hope Slide. Alders reflect the high moisture content along wetlands of the Sumallo River southeast of Tashme.

Where valley walls are heavily vegetated and mantled with colluvium outcrop is scarce, however outcrop is exposed in cliff faces and along the walls of large gullies or gorges cut in the valley walls by post-glacial erosion. Well developed joint faces and cliffs are observed on both sides of the valley and talus and scree deposits are common. Fluvial deposits of sand and gravel

blanket the valley floor. These deposits are intermixed with alluvium and debris flow deposits associated with an erosional scar on the northeast valley wall between the Hope Slide and Tashme. The scar was probably formed through the post-glacial processes of stream erosion, rockslide and avalanche erosion.

Wetlands are common in the valley and include cedar swamps, alder swamps and grassy wetlands.

### Bedrock Geology

The understanding of the causes and prediction of geological hazards is dependent upon an understanding of the geological history of an area and the resultant geology. Important factors to be considered in bedrock geology are:

- rock type which defines the strength of a rock; stratigraphy which defines potential planes of slippage; and structure which defines potential planes of weakness including fault or joint systems.

Rocks of igneous, metamorphic, sedimentary and volcanic origin exist within the present metamorphic complex of Sunshine Valley. The oldest rocks mapped in the area are upper Paleozoic rocks of the Hozameen Group which is composed of greenstone, chert, argillite, pelite and minor limestone. The dip of the bedding is variable and ranges from 40 to 65 degrees to the southwest. Greenstone is the dominate rock type on the northeast wall of the valley. These rocks are overlain stratigraphically by well bedded chert and pelite not observed on the northeast valley wall but exposed on the southwest flank of the valley south of Tashme. The chert horizon is overlain by a unit containing metavolcanic rocks, chert, argillite and limestone. This unit is exposed on the southwest flank of the valley, north of Tashme.



Rocks of the Hozameen Group have undergone low-grade metamorphism (lowest greenschist facies) and are complexly folded and faulted.

Felsite dykes and sheets up to 20 ft. in thickness appear to have intruded planes of weakness such as joints or bedding in Hozameen rocks and consequently, where observed, show variable dips. Open joints or tension fractures are well developed in all rocks of the Hozameen Group.

Cretaceous quartz diorite has intruded the Hozameen Group rocks and is exposed at the tops of the peaks along the northeast valley wall as shown in Figure 2.

#### GEOLOGICAL HAZARDS

The various geological hazards defined in Sunshine Valley are numerous and show much variety in degree of hazard and of character. Where possible similar types of hazard have been grouped and numbered for ease of reference as illustrated in Figure 3. In the following paragraph each hazard site will be considered with respect to: notation, general location, description and recommendations.

##### Hazard Site 1 - Hope Slide Debris

###### Location

Area north of LS<sup>1</sup> 6,7,8, S<sup>2</sup>12, R<sup>3</sup>25, T<sup>4</sup> and all legal subdivisions north of this area.

###### Description

The area occupied by debris from the Hope Slide is defined in Figure 3. A detailed description of this catastrophic landslide is provided by Brawner (1975) and Mathews and McTaggart (1969). Figure 4 illustrates a mound of blocky rubble which blankets an abandoned portion of Provincial Highway 3.

### Recommendations

The report by Nasmith (1977) recommends "that no permanently occupied structures be built on ground deposited or disturbed by the Hope Slide".

This statement implying that the slide debris represents a high hazard area is accepted and the recommendation is presented that no structures intended for habitation should be constructed within the limits of ground on which slide debris has been deposited.

### Hazard Site 2 - Falling and Sliding Rock Hazard

#### Location

LS15, S1, R25, T4

This hazard site encroaches upon lots in Cedar Village Subdivision (approved 1971). All of Lots 83 and 82 are located within a hazardous zone and portions of Lots 85, 81 and 80 are subject to a high degree of falling rock hazard.

#### Description

Geological hazard site 2 includes an area subject to falling and sliding rock hazard. Angular blocks have fallen from a well jointed outcrop and cliff exposure forming the valley wall above the lots referred to. Since abundant evidence of ancient and fresh rockfalls was observed the zone may be classified as a high hazard zone. Figures 5 and 6 show recent rockfalls which have fallen from the cliff face to steep slopes below the cliff. The blocks are sufficiently large to do extensive damage or injury to man and structures. Both of the photographed blocks rest upon forest debris. Above these falls trees have been broken and fresh scars were observed on slopes and trees where large rock blocks had fallen from the cliff face and on the downward course had removed surficial vegetation from the soil and had scraped bark from trees.

### Recommendations

A zone of high hazard was determined by defining a reasonably safe limit of travel for falling, rolling, sliding or bouncing rocks as indicated by the position of the limit of occurrence of previous rockfalls. This zone is represented in Figure 7. Since this zone is an active hazard zone for large falling blocks the restrictions on development are readily defined:

- Lots 82 and 83: - No habitable structures should be erected on these lots.
- Lots 80, 81 and 85: - No habitable structures should be erected southwest of the defined setback zone.
  - No trees should be removed from slopes within the hazard zone.

### Hazard Site 3 - Falling Rock Hazard

#### Location

The hazard zone is located southeast of Tashme in LS3, 4 and 5, S6, R24, T4.

#### Description

Large open, vertically dipping to subvertical joints have developed on a cliff face southwest of Tashme. Large angular blocks lie at the base of the cliff from which they have fallen. Some large blocks are also perched on ledges on the cliff face. Figure 8 illustrates a portion of the cliff face and large open tension fractures. One large rock block may be discerned on a ledge. In view of the open nature of the joint system it is obvious that weathering processes will continue to separate and loosen large rock slabs peculiar to this hazard zone. Consequently, it is expected that rockfalls will continue to pose a hazard along the base of the cliff.



### Recommendations

In view of the active nature of this rockfall hazard and the great momentum that falling rocks of large size may attain, it is advised that no habitable structures be erected within two hundred feet from the base of the cliff face.

Since it is difficult to define exact boundaries of landforms from aerial photographs it is recommended that if any development is proposed near the hazard zone a survey plan should be submitted to the approving officer. The plan should define the slope at the base of the cliff indicated to be a hazard site and reference metal bars should be located in the field northeast of the cliff face.

### Hazard Site 4 - Rockslide Hazard

#### Location

The hazard site is located above the now abandoned site of Sumallo Lodge, LS1, S6, R24, T4.

#### Description

Talus and scree slopes occur along the lower valley walls at this hazard site (Figure 3). Ravelling and small rockslides occur throughout this hazard zone. This is primarily a result of the development of planes of weakness along bedding and some joint faces which, on this side of the valley dip downwards towards the valley bottom.

### Recommendations

It is recommended that no development occur within 200 ft. of the toe of the slope contained within hazard site 4. If development is proposed near the boundaries of this hazard zone, the developer should submit a survey plan precisely defining the location of the development with respect to the toe of the slope.

Hazard Site 5 - Debris Flow Hazard

Location LS8, S6, R24, T4

Description

On the south side of Johnson peak a large rapidly flowing stream exits from a large drainage basin to traverse a well developed alluvial fan. The stream flows down the median of the fan and discharges into the Sumallo River on the south side of Provincial Highway 3. The alluvial fan is composed of poorly sorted sand, gravel, cobbles and boulders. Large boulders and forest debris distributed along the stream channel attest to the energy potential of the stream (Figure 9). Large blocks of rock are distributed on the surface of the fan below the apex. Since these blocks are surrounded by mature trees the blocks are likely prehistoric in age. Ground and aerial photograph examination of the fan indicates that near the apex the fan is dissected by large gullies, tributary and distributary stream channels. In view of the present flow rate and quantity of the water reaching the base of the fan and the presence of large gullies, tributaries and distributaries near the apex of the fan, it is judged that, in its upper reaches, the stream has considerable erosive capabilities. Consequently, the ground downstream from the apex of the alluvial fan may in the future be subject to debris flow hazard.

Recommendations

In consideration of the possibility of debris flows occurring throughout the above mentioned alluvial fan, a zone has been defined (Figure 3) surrounding the main stream channel, tributaries, distributaries and gullied terrain. Any planned development within this area should require a contoured survey plan, at a scale of one inch equals fifty feet. In order to provide a reasonable measure of



safety from debris flows and channel bank erosion, lot setback limits of 200 ft. should be applied to the boundaries of the above mentioned erosional features: stream channels and gullies. The fan and the upstream drainage basin are vegetated with mature stands of conifers. If this cover is removed erosive processes will increase promoting the development of debrisflows. Therefore, it is recommended that logging operations be strictly controlled within the drainage basin of the stream.

#### Hazard Site 6 - Rockfall Hazard

Location Northeast valley walls opposite Tashme. LS6, S7, R24, T4.

#### Description

Non-vegetated outcrop exposures along this northeast valley wall are strongly jointed and prone to small scale landslides. This is a result of the structure of the bedrock on this flank of the valley. On this northeast wall bedding and some joint surfaces dip downward to the southwest; consequently, erosional processes including avalanching and freeze-thaw processes promote dislodgement of rock from the valley wall.

#### Recommendations

This hazard zone is well above any area which is presently considered suitable for development. Falling rock debris is presently retained by dense forest growth on the lower slopes. In its present state, the falling or sliding rock from this zone poses no danger to development in the valley lowlands. However, localized falling rock hazard may occur on the slopes below this zone. Consequently, any development on the valley walls below this zone should require submission of detailed lot plans for evaluation by the approving officer.

## Hazard Site 7 - Rockfall and Rockslide Hazard

### Location

A rocky gorge 0.8 miles southeast of the southeast side of the Hope Slide located in and above LS8, S12, R25, T4.

### Description

Figure 11 illustrates a rock walled gorge on the northeast flank of Sunshine Valley. Few mature trees grow on the floor of the gorge in its upper regions. Well-jointed, near vertical cliffs characterize the northeast wall of the gorge (Figure 12). At the base of the gorge a debris fan is present, composed of alluvial deposits and large angular and subangular blocks which are embedded in or lie on the surface of the debris fan. The geology on this side of the valley is similar to that observed at the site of the Hope Slide. Bedding of the greenstones in this area dips toward the valley floor; jointing is well developed with some joint surfaces dipping towards the valley floor.

### Recommendations

It is the opinion of the writer that in view of the similarity in geology between this site and that of the Hope Slide that some major rockslide hazard does exist in the vicinity of the gorge and its extension. Since the gorge itself is a narrow and well defined zone subject to falling, bouncing or sliding rock hazard development below the cliffs and within the zone defined in Figure 3 should be prohibited.

Hazard Site 8 - Rockfall and Rockslide Hazard

Location

IS10, 11, 14, S12, R25, T4.

This group of hazard sites is located immediately southeast of the Hope Slide.

Description

As illustrated in Figure 14, this hazard group is associated with strongly jointed outcrop exposures in the lower valley walls adjacent to the Hope Slide. Large talus and scree deposits occur at the base of these slopes.

Recommendations

No development should be allowed within 200 feet of the base of these slopes. If development is initiated near these slopes detailed development plans should be submitted to the approving officer for evaluation. Reference stakes should also be implanted in the field to define slope boundaries and to locate proposed buildings.

Hazard Site 9 - Rockslide Hazard

Location S12, S13, R25, S18 R24, T4

Description

These hazard zones include areas of outcrop associated with talus and scree slopes. All such zones are high on the valley walls.

Recommendations

In view of the remoteness of these zones and the well timbered slopes below, they pose no threat to development on the valley lowlands.



#### AVALANCHE HAZARD

The limit of active and potential avalanche sites in the Sunshine Valley area was defined by G.L. Freer, Senior Avalanche Co-ordinator. His comments on avalanche sites are included in the appendix. Figure 15 illustrates the extent of active and passive avalanche sites.

No development should occur on any areas designated as active avalanche sites. As G. Freer points out "should the forest cover be lost due to development, fire or logging, many active avalanche sites would appear". Therefore, it is recommended that any logging proposed within potential avalanche terrain should be strictly controlled. In compliance with the above observations, it is recommended that in zones of active and passive avalanche terrain that developers of property should not remove trees other than at approved building sites and access routes.

It is notable that most avalanche sites occur on the northeast side of the valley and that many of these sites overlap sites of geological hazard. This has practical significance as slopes on the southwest side of the valley are generally steeper than those on the northeast side of the valley. One important factor which relates the coincidence of geological and avalanche hazard sites is the structure of the rock units forming the northeast valley wall. Bedding of the greenstones, which are mapped along the northeast valley wall, dips toward the southwest. These greenstones are well jointed and intruded by felsite sheets. The resultant planes of weakness along this valley wall have resulted in the development of more numerous geological and avalanche hazard sites than occur on the southwest side of the valley where bedding and joint planes dip into the valley slope.

### CONCLUSIONS

This report depicts the nature and extent of geological and avalanche hazards in the Sunshine Valley area. Recommendations with respect to development on sites exposed to avalanche hazard are straight-forward and indicate that no development should occur at "active" avalanche sites and that restricted development may occur in potential avalanche terrain.

Geological hazards are numerous throughout the valley and of a varied nature. Therefore, where possible, specific recommendations have been presented for each hazard site. Where specific recommendations are not possible due to complexity or where physical location of topographic features is not precise, recommendation guidelines are presented for the assistance of the approving officer.

During the study it was observed that the geological structure promotes a greater instability of outcrop surfaces on the northeast valley wall than on the southwest valley wall. This has resulted in a coincidence of avalanche and geological hazard sites along the northeast valley wall.

In view of the broad range of hazards, the coincidence of hazard sites and the lack of precision in defining hazard zones from relatively small scale aerial photographs, the recommendation is presented that any proposed development near hazard zones require submission, by the developer, of detailed plans of development. These plans should be submitted to the appropriate Ministry of Highways Approving Officer. In addition, where any ambiguity with regard to location of zone or landform boundaries exist, such boundaries should be clearly staked in the field by the subdivider or his agents and defined by standard iron bars.



The lowlands of the valley are occupied by large areas of wetlands and floodplain. Since this implies the possibility of flood hazard in certain sections of the valley floor it is recommended that representatives of the Ministry of the Environment examine the valley lowlands before development is approved in such areas.

#### REFERENCES

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- (2) Mathews, W.H. and McTaggart, K.C., 1969, The Hope Landslide, British Columbia, Geological Association of Canada Proceedings, Volume 20, pp. 65-75.
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- (4) Monger, J.W.H., 1970, Hope Map Area, West Half, British Columbia, Geological Survey of Canada, Paper 69-47.
- (5) Nasmith, H.W., 1977, Overview of Reports on Natural Hazards, Sunshine Valley Subdivision, Thurber Consultants Ltd. Report.

APPENDIX

Comments by G.L. Freer, Senior Avalanche Co-ordinator  
on  
Sunshine Valley Snow Avalanches

The attached map and air photo outline active (red) and potential (blue) snow avalanche sites in the Sunshine Valley area.

Active avalanche sites are those which presently have snow avalanches occurring most winters.

Potential sites should also be considered. Most of the mountain terrain in this area is steep and well forested. Should the forest cover be lost due to development, fire or logging, many active avalanche sites would appear.

If in the future, developments are proposed near these avalanche sites, studies should be initiated to establish more specific zoning boundaries than is allowed by the present mapping and time constraints.

*G.L. Freer / L.S.*

G. L. Freer  
Senior Avalanche Co-Ordinator

GLE/lis

Encl.

cc: Director Maintenance Services  
Senior Approving Officer

Figure 1 - Sunshine Valley property limits.

Townships (T), Sections (S), Range (R)  
and legal subdivisions are identified  
by number (after Figure 2, Brawner et.  
al., 1975).



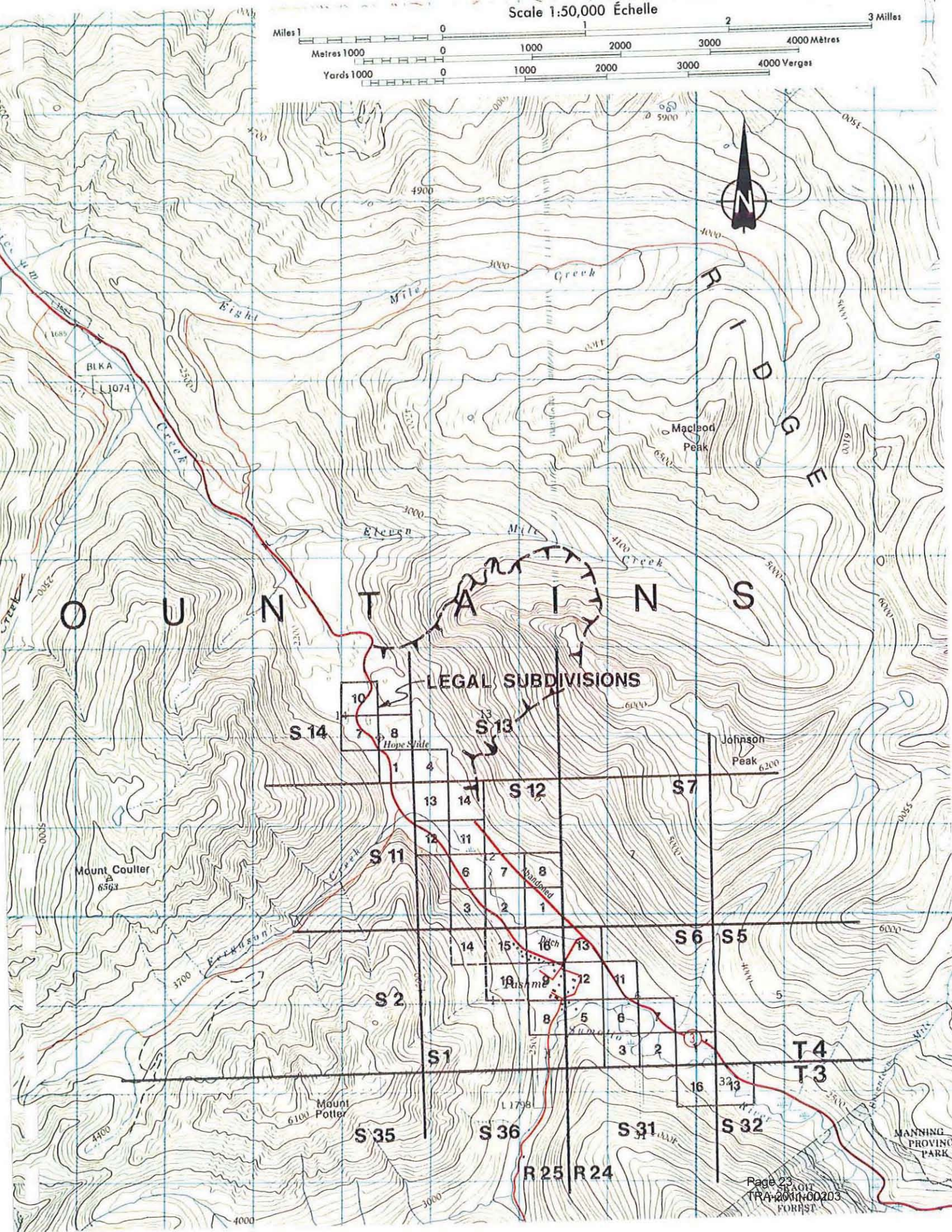
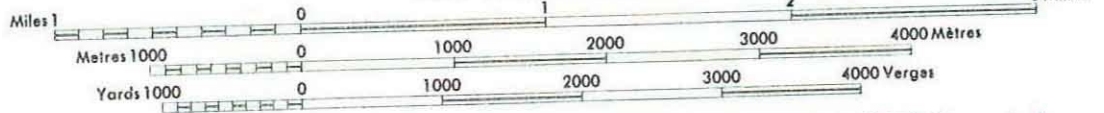
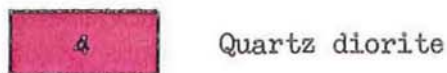




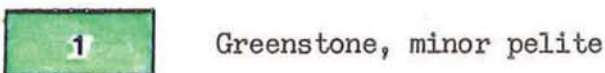
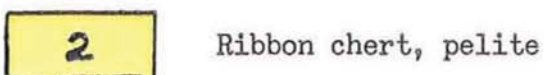
Figure 2 - Bedrock geology of the Sunshine Valley area. Compilation from Brawner et.al., 1969; Mathews and McTaggart, 1969; Monger, 1969.

EXPLANATION

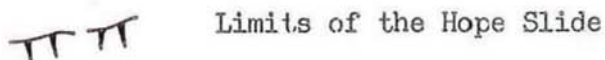
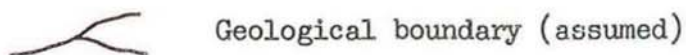
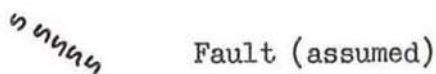
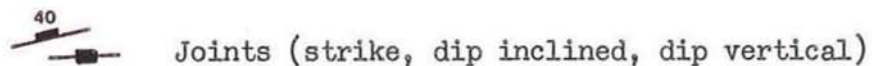
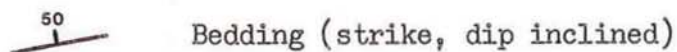
Cretaceous



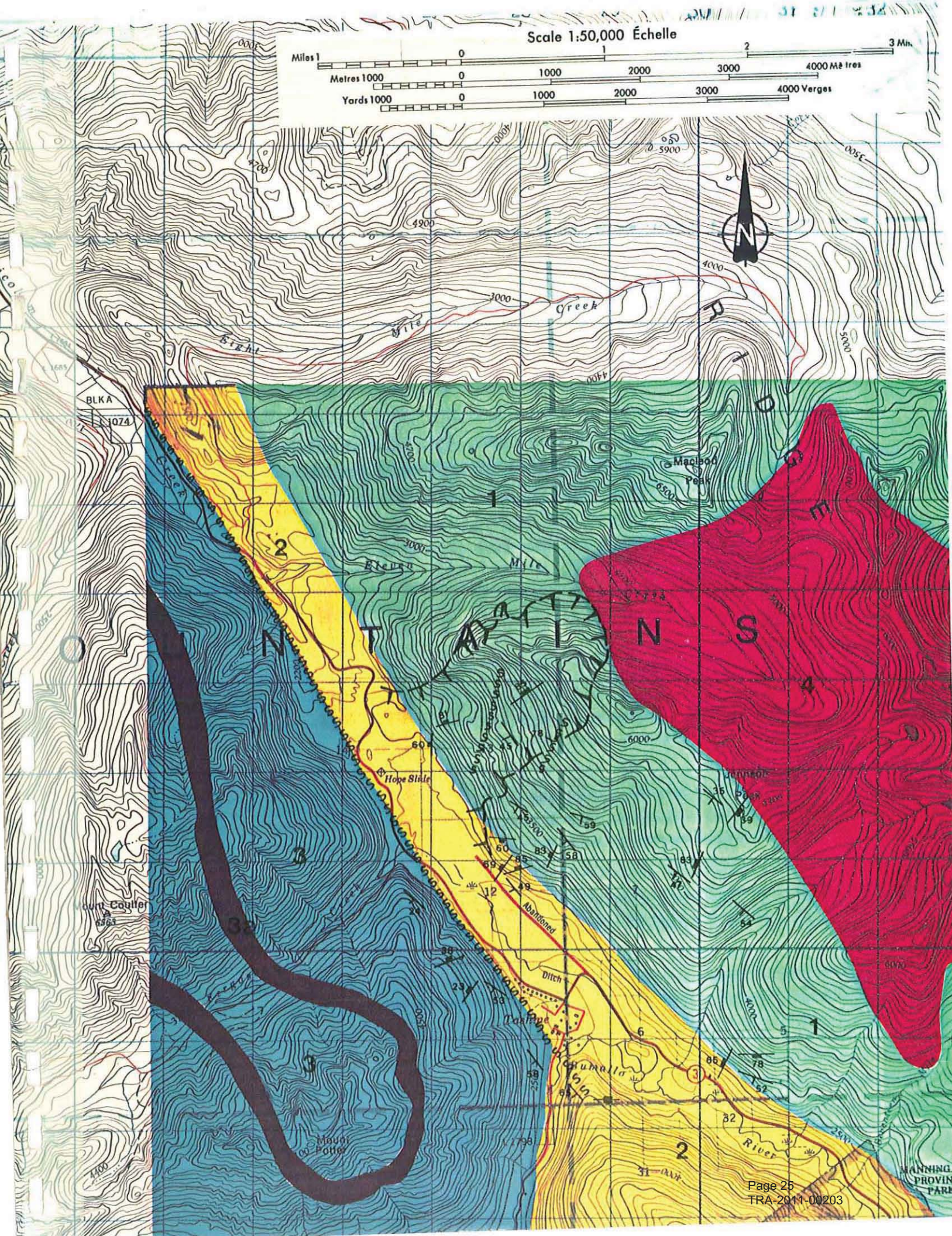
Upper Paleozoic [Devonian (?), Carboniferous (?), and Permian (?)]



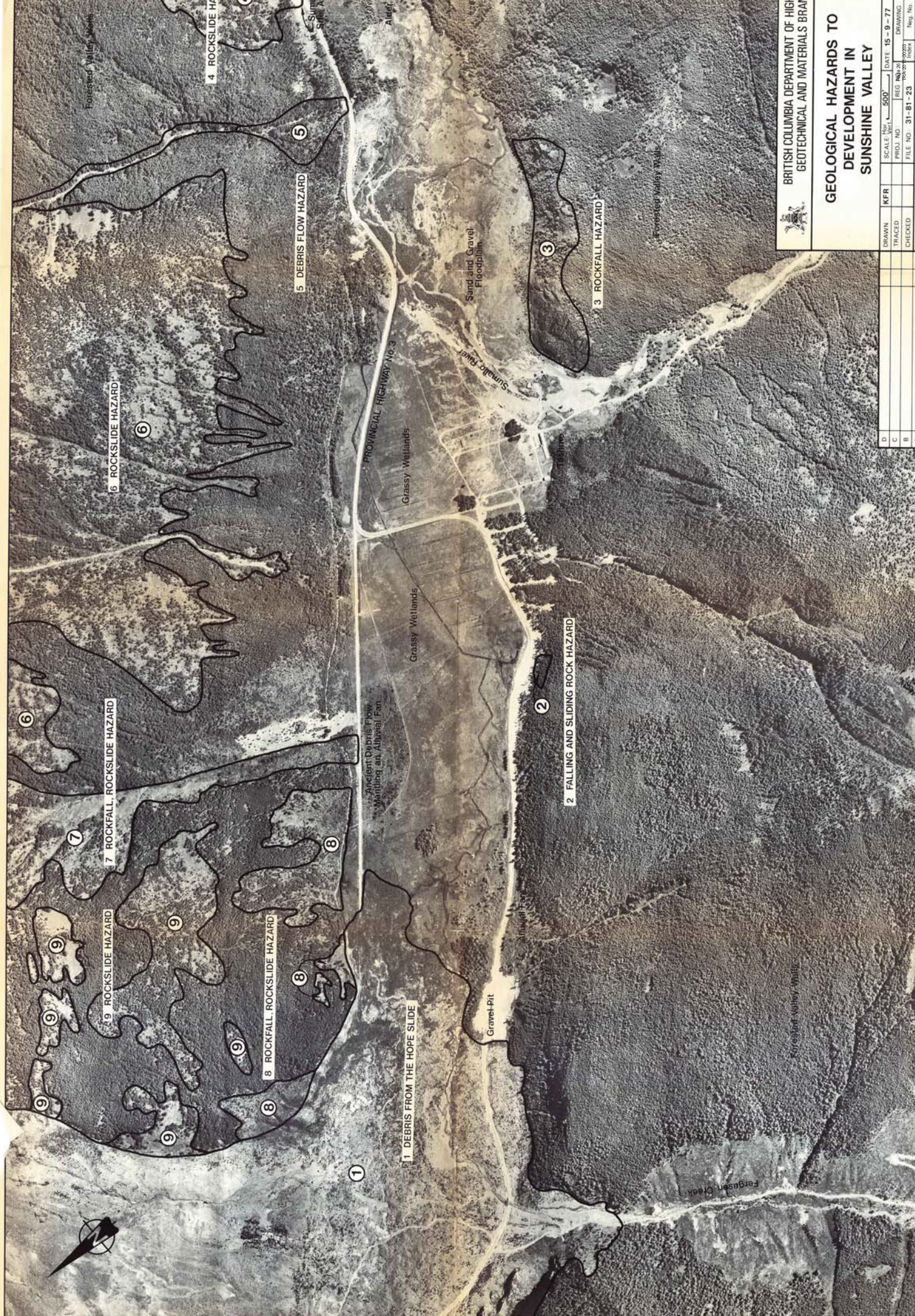
SYMBOLS











BRITISH COLUMBIA DEPARTMENT OF HIGHWAYS  
GEOTECHNICAL AND MATERIALS BRANCH

**GEOLOGICAL HAZARDS TO  
DEVELOPMENT IN  
SUNSHINE VALLEY**

DRAWN	KFR	SCALE	500'	DATE	15-9-77
TRACED		PROJ. NO.		HIGH. NO.	00200
CHECKED		FILE NO.	31-81-23	INDEX	
				NEAT NO.	





Figure 4 - Geological hazard site 1. Slide debris covering the old highway.

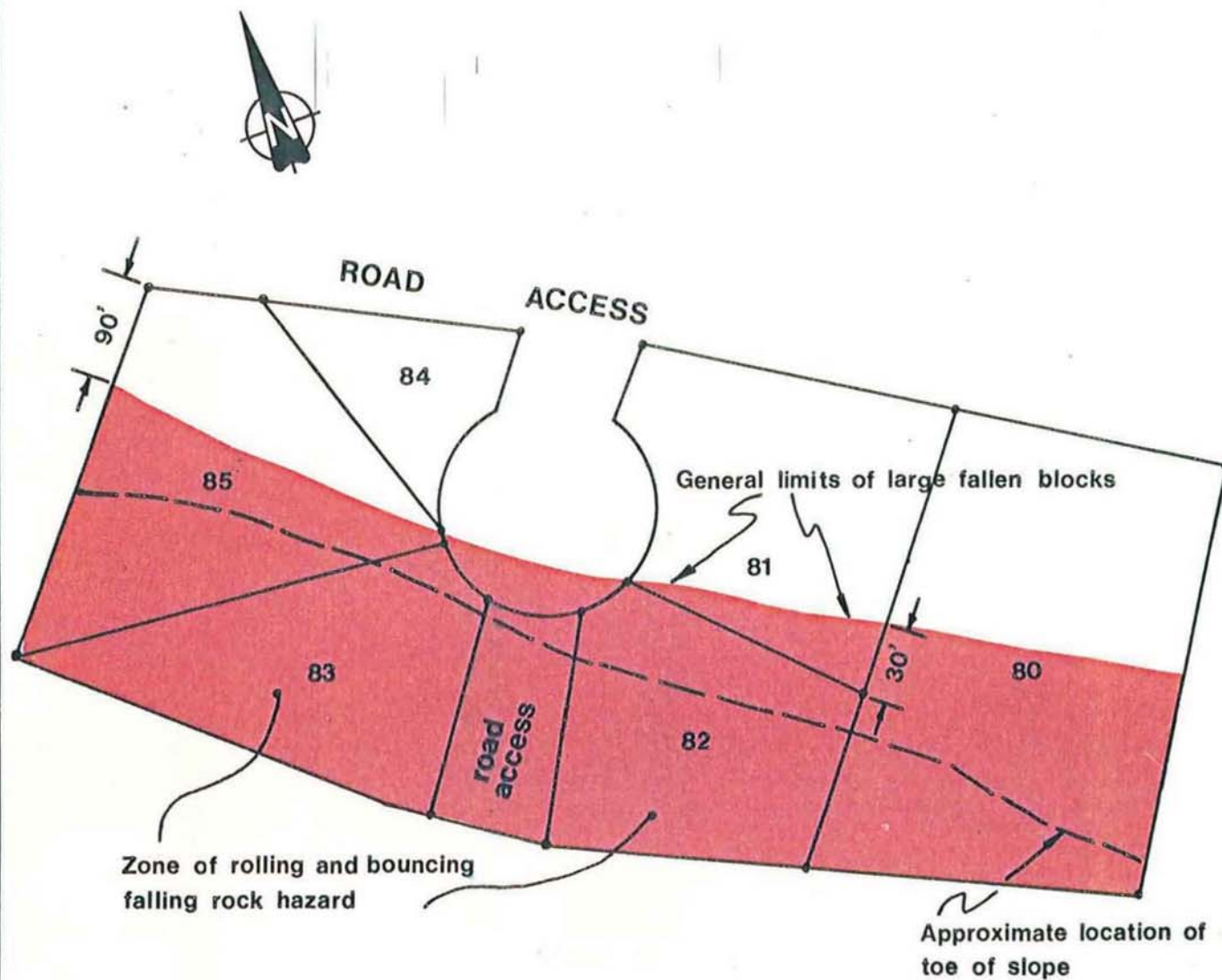




Figure 5 - Geological hazard site 2. A recent rockfall on a decayed tree stump.



Figure 6 - Geological hazard site 2. A recent rockfall.



**SKETCH ILLUSTRATING A  
FALLING ROCK HAZARD ZONE**

BRITISH COLUMBIA DEPARTMENT OF HIGHWAYS  
GEOTECHNICAL AND MATERIALS BRANCH

**FIGURE 7**

D	DATE	SCALE	FILE NO.	DATE
C				
B				
A				
REVISIONS		DRAWN TRACED CHECKED APPROVED PROJECT NO. FILE NO.		

Page 29  
TRA-2011-00203

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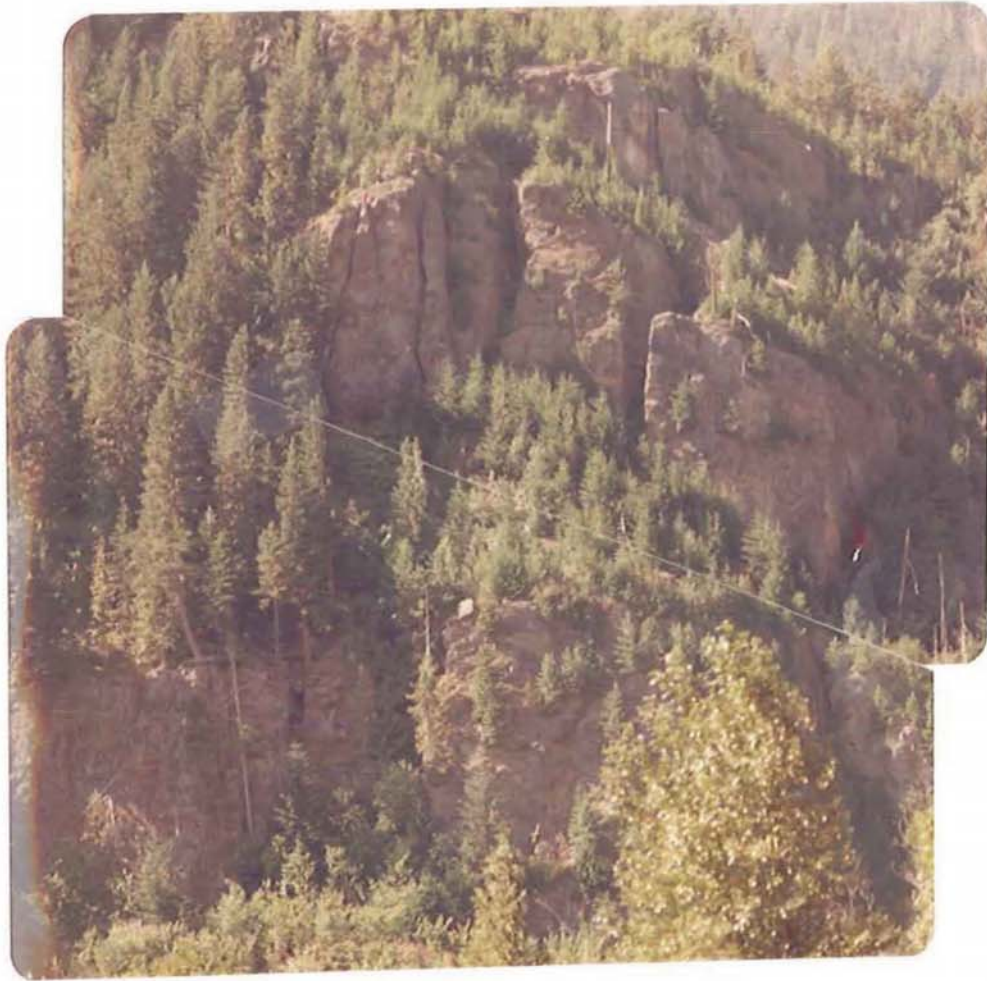


Figure 8 -- Geological hazard site 3. Note the open joints.



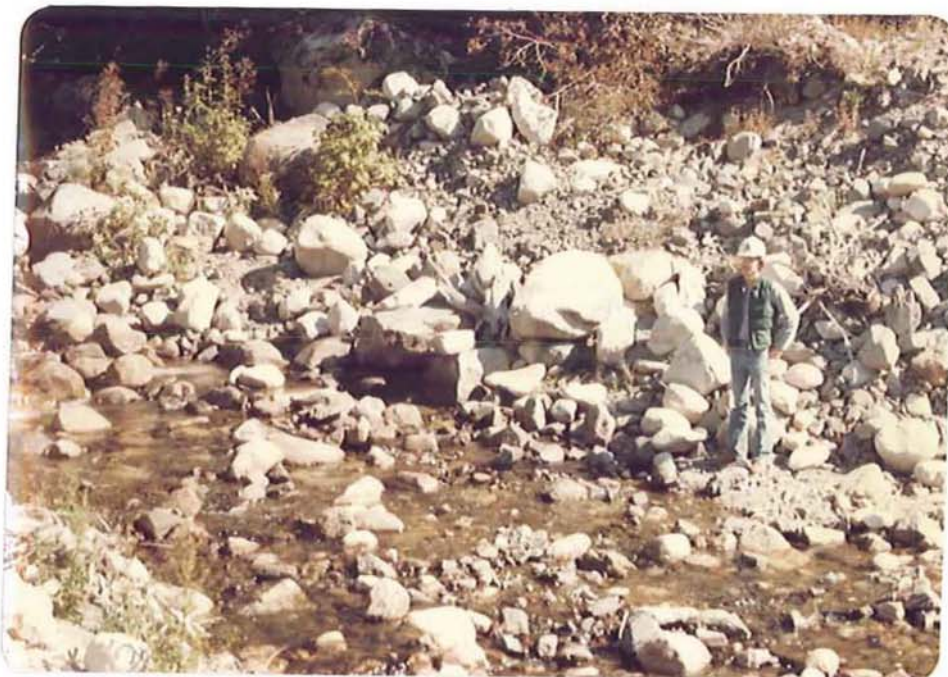


Figure 9 - Geological hazard site 5. The main stream channel of the alluvial fan.



Figure 10 - Geological hazard site 5. A large rock block on the surface of an alluvial fan.



Figure 11 - Geological hazard site 7. The upper area of this chute is an active avalanche site.

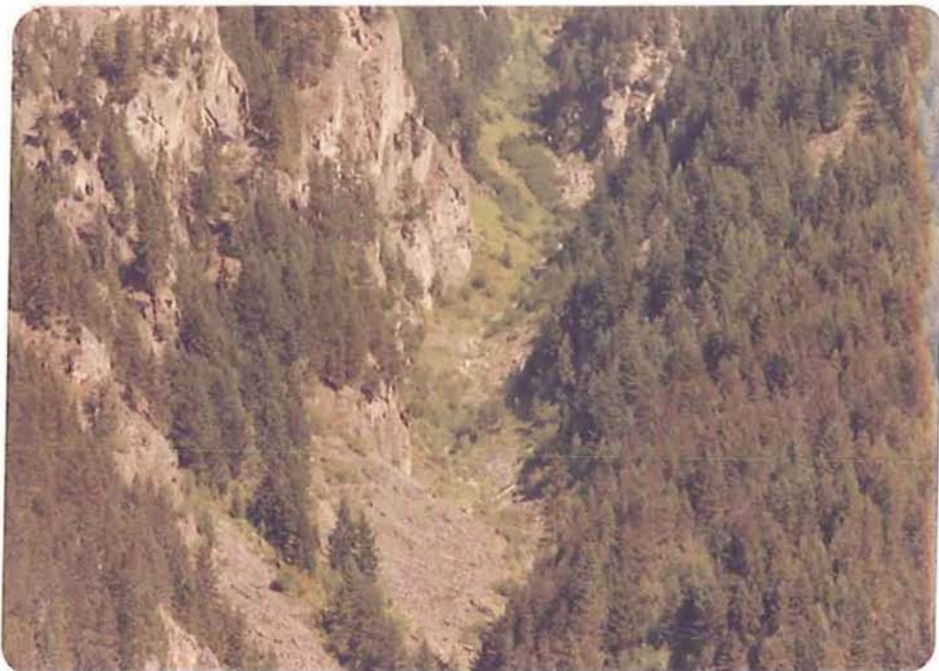


Figure 12 - Geological hazard site 7. Note the well-jointed near-vertical cliff faces and talus slopes.





Figure 13 - Geological hazard site 7. A large block of rock carried to this position by large landslide or debris flow.



Figure 14 - Geological hazardsite 8. Blocky talus and well developed joint systems.



APPENDIX

Comments by G.L. Freer, Senior Avalanche Co-ordinator  
on  
Sunshine Valley Snow Avalanches

The attached map and air photo outline active (red) and potential (blue) snow avalanche sites in the Sunshine Valley area.

Active avalanche sites are those which presently have snow avalanches occurring most winters.

Potential sites should also be considered. Most of the mountain terrain in this area is steep and well forested. Should the forest cover be lost due to development, fire or logging, many active avalanche sites would appear.

If in the future, developments are proposed near these avalanche sites, studies should be initiated to establish more specific zoning boundaries than is allowed by the present mapping and time constraints.

*G.L. Freer / L.S.*

G. L. Freer  
Senior Avalanche Co-Ordinator

GLF/lis

Encl.

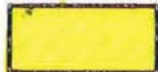
cc: Director Maintenance Services  
Senior Approving Officer

Figure 15 - Sunshine Valley Avalanches

Explanation



Active avalanche sites: Those sites which presently have snow avalanches occurring most winters.



Potential avalanche terrain: If the vegetation were removed by development, fire or logging, active avalanche sites would develop.

Symbols



Provincial Highway #3



Secondary road



Track

Scale

1 inch equals 1455 feet









H118

To: K. Bessflug, P. Eng.  
Regional Geotechnical and Materials Engineer  
Burnaby

Date: 1979/05/10

Headquarters File: 31-81-23

Regional File:

District File:

Re: Sunshine Valley

Enclosed are the requested copies of the two reports  
on Sunshine Valley by: 1) Golder Brawner and Associates and  
2) Thurber Consultants.

*D. R. Haughton*

D.R. Haughton, P. Eng.  
Special Projects Engineer

for:

Director of Geotechnical  
and Materials Engineering

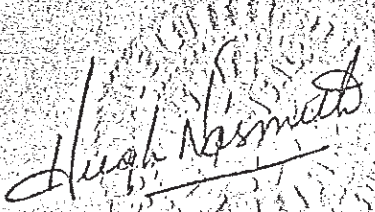
DRH/jj



OVERVIEW OF REPORTS ON NATURAL HAZARDS  
SUNSHINE VALLEY SUBDIVISION

PREPARED FOR  
MINISTRY OF HIGHWAYS OF BRITISH COLUMBIA

August 5, 1977  
File No. 15-3-27

  
H.W. Nasmith, P.Eng.  
Thurber Consultants Ltd.  
Victoria, B.C.

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## APPENDIX

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## S Y N O P S I S

This report presents an overview of reports by various authorities on natural hazards which may affect the Sunshine Valley Subdivision. The principal concern relates to the risk of a repetition of the Hope Slide which affected part of the property owned by Sunshine Valley Developments Ltd.

The risk of another catastrophic rockslide on the site of the Hope Slide is probably comparable to the risk of other natural hazards (earthquakes and floods) accepted in other parts of British Columbia. The risk of a catastrophic rockslide from the valley walls where no previous rockslide has occurred are believed to be several orders of magnitude smaller.

It is recommended that no permanently occupied structures be built on ground deposited or disturbed by the Hope Slide and that subject to assessment of the hazard of snowslides and rolling boulders, development can be allowed of the remaining portion of the proposed subdivision.

## INTRODUCTION

The study on which the following report is based was requested by Mr. M.G. Elston, P.Eng, Executive Director Engineering, Ministry of Highways and Public Works. The work was authorized in a letter dated June 14, 1977 from Mr. S.E. Price, Director of Financial Services.

In the course of the study the documents listed in the Appendix were reviewed and the problem was discussed with Mr. C.O. Brawner of Golder Associates and with Dr. W.H. Mathews. In addition the writer made a ground and helicopter reconnaissance of the Sunshine Valley Subdivision in the company of Mr. J. Horcoff of the Ministry of Highways.

## PURPOSE & SCOPE OF THE STUDY

The purpose of the study as requested by Mr. Elston is to provide an overview of the potential natural hazards of the Sunshine Valley Subdivision Development.

The Sunshine Valley Subdivision is located in the bottom of the narrow steep sided valley which forms the divide between the Sumallo River drainage and Nicolum Creek on Highway 3. about twelve miles east of the town of Hope. The Hope Slide is located at the western end of this valley and debris from the slide was deposited on some of the property owned by Sunshine Valley Developments Ltd.

Concern regarding the safety of the Sunshine Valley Subdivision has been expressed because of the statement in the paper by Mathews and McTaggart that the authors "would not care



to live at the foot of this same hillside and can recommend that no permanent inhabited structures be established along this stretch of the valley". However, in a letter to the Ministry of Highways dated February 21, 1977, Dr. Mathews has indicated that he cannot precisely define the stretch of the valley involved in this statement.

Golder Associates have prepared a report for Sunshine Valley Developments Ltd. dated February 1975 based on a geotechnical investigation of the stability of the Sunshine Valley slopes. This report is principally concerned with the likelihood of future, large-scale slope failures similar to the 1965 Hope Slide but also identifies the presence of snowslides and minor falls of loose boulders.

The following statements from the Golder Associates report appear to represent their conclusions:

p.2 "We have discovered no evidence suggesting that the slopes above Sunshine Valley southeast of the Hope Slide have an abnormal tendency to be eroded through major landslides and therefore development within this portion of the valley appears to be subject to no unusual degree of risks."

and "We recommend that no permanently inhabited  
p.4 structures be developed on the ground deposited or seriously disturbed by the 1965 landslide."

The principal concern addressed in this overview study is the significance of the Hope Slide relative to the Sunshine Valley Sub-division development. A brief comment will be included on the hazard from snowslides and minor

falls of loose boulders identified in the Golder Associates report.

#### HOPE SLIDE

The Hope Slide has been described in the paper by Mathews and McTaggart. The abstract of their paper is quoted in full in the following paragraphs:

"Early on January 9, 1965, a landslide of about 130 million metric tons of rock buried British Columbia Highway 3 to a maximum depth of 260 feet, between miles 10.3 and 12.0 southeast of the town of Hope in the Cascade Mountains.

The landslide incorporated massive to slightly schistose green metavolcanics with intrusive sheets of felsite which dipped nearly parallel to the 30° slope of the mountain side. One prehistoric major landslide and a similar incipient slide have been recognized on the same slope. Geological conditions contributing to the slides include: weakness in or adjacent to the felsite sheets, schistosity and jointing parallel to the surface, and local serpentine alterations and carbonate and zeolite joint fillings. Geological processes, e.g. solution or weathering, leading to a progressive weakening seem, however, unimportant. Meteorological and hydrological conditions seem also to have had no importance in initiating the 1965 slide.

Two earthquakes of magnitudes 3.2 and 3.1 were associated with snowslides and the major landslide and are considered to have been a major factor in launching them. However, a much larger earthquake in this



area in 1872 apparently had little effect. Contributing seismic factors to the 1965 landslide may have been shallow focal depth, appropriate frequency, duration and orientation.

Course rocky detritus, originating principally from the slope 2,200 to 3,600 feet in elevation above its foot, slid, apparently in surges, and came to rest on the valley floor in a series of concentric ridges displaying diffraction effects at their lateral terminations. A mixture of rock debris and mud, the latter derived from the valley floor, was driven vigorously ahead and rebounded from the opposite valley wall. Most came to rest on or adjacent to the rockslide debris. Effects of air-blast and a cloud of airborne dirt extended beyond the limits of the rock and mud slides. Landslide energy,  $10^{22}$  ergs, was dissipated mostly as heat".

Mathews and McTaggart initiated their study of the Hope Slide within a few days of the event and accordingly were able to record ephemeral features such as airblast effects and the distribution of trees on the surface of the slide debris. Vertical airphotographs of the site both before and after the slide were available and topographic maps for comparative purposes were prepared. Seismic records were available and have been analysed. Many geologists have made more or less detailed observations at the site since 1965.

There is probably a greater amount of reliable factual data available for the Hope Slide than for any other major rockslide in North America and probably in the world. In spite of this wealth of data, information on

the geologic structure of the mass of rock that moved, on the groundwater conditions prevailing at the time of the slide, and on the sequence of events and rate of movements are not known and can only be inferred from observations made after the slide took place.

These factors (geologic structure, groundwater conditions, rate and sequence of movements) are critical to an analysis of the slide, and since they must be inferred from limited data it is to be expected that equally competent and experienced geologists and rock mechanics specialists can arrive at differing and even contradictory interpretations of the critical factors which governed the occurrence of the slide. When these interpretations of the critical factors are extrapolated to assess the stability of a natural rock slope which has not yet failed it is clear that the conclusions may be contradictory.

In the following paragraphs an approach which differs from the geologic/analytic approach is discussed and which it is believed may be useful in assessing the risk presented by conditions similar to those in the Sunshine Valley.

#### CATASTOPHIC ROCKSLIDES

In geologic terminology a catastrophe is "a sudden violent change in the physical conditions of the earth's surface" and accordingly the Hope Slide could be described as a catastrophic rockslide. Catastrophic rockslides could be defined as those rockslides in which a large volume of rock moves rapidly with no warning. Within historic times in Western Canada the Rubble Creek Slide, the Frank Slide and Devastation Glacier Slide would be classified as



"catatrophe rockslides". The Downie Slide and the Drynock Slide would not be classified as catastrophic because, although they involve very large volumes of rock, they move slowly at rates measured in feet per year rather than feet per second.

Catastrophic rockslides are a widespread occurrence in both time and space in a mountainous region. The evidence for a catastrophic rockslide will persist and can be recognised by an experienced observer for thousands of years after the event. This contrasts with other geologic catastrophes such as earthquakes and floods.

In spite of their widespread occurrence catastrophic rockslides probably account for a very small percentage of the material transported by other mass wasting processes in a mountainous region. They must be regarded as a rare event, and would not be of interest if it were not for their spectacular and devastating consequences.

The features of a catastrophic rockslide which make it of concern in the development of permanently occupied structures are that they occur without warning and that the devastation is complete. In contrast to most floods, there is no opportunity to recognise the onset of the hazard and evacuate the threatened area; and in contrast to earthquakes, no expenditure on a structure can improve its ability to resist the effects of a catastrophic rockslide.

The only protection from a catastrophic rockslide is to avoid the location where the slide occurs. This leads to a consideration of the risk of a catastrophic rockslide at any specific location and is discussed in the following paragraphs.

## RISK OF A CATASTROPHIC ROCKSLIDE

In spite of the widespread occurrence of catastrophic rockslides in a mountainous region they must be regarded as rare events in the human scale. A catastrophic rockslide will only occur where an exceptional or almost unique set of conditions exists. A rock mechanics engineer would find it an impossible task to design a rock slope which would stand for thousands of years before suddenly collapsing. Therefore, although we may not be able to analyse in detail the complex conditions leading to a catastrophic rockslide, the fact that a catastrophic rockslide has occurred at a locality proves the existence of an exceptional combination of fixed geological conditions (rock strengths, fractures, structures, permeable and impermeable zones, etc.) which when acted upon by variable conditions (weathering, freezing and thawing, groundwater fluctuations, seismicity, etc.) ultimately resulted in a catastrophic rockslide.

In some cases the catastrophic rockslide will remove one or more of the key conditions so that no further rockslides occur. However, in many instances the catastrophic rockslide exposes a continuation of the critical geologic conditions and sets the stage for another catastrophic rockslide. At the Rubble Creek Slide, the Hope Slide, the Devastation Glacier Slide and the Lake of the Woods Slide there is evidence of more than one catastrophic slide at the same site. Therefore although catastrophic rockslides are rare events and the risk of a slide at any specific time is small, the risk at a site where a catastrophic slide has occurred in the past is of a different order of magnitude (possibly a thousand times as great) when compared to the risk at a location where no catastrophic slide has ever occurred.



In British Columbia the Pleistocene glaciers scoured and eroded the land surface and when they melted away about 10,000 years ago many of the valley slopes were left oversteepened and rock conditons were exposed which had never been exposed before. Catastrophic rockslides which are recognised in British Columbia have all taken place since the end of the ice age 10,000 years ago. Many of these rockslides probably occurred shortly after the glaciers melted away but they nevertheless indicate the presence of geologic conditons favourable to the development of a catastrophic rockslide. Sites where catastrophic rockslides have occurred during postglacial time must therefore be regarded as subject to much greater risk (however small it may be) than sites where no catastrophic rockfall has occurred.

#### ACCEPTABLE RISK

Everyone who lives or travels in a mountainous region is subject to some degree of risk however small from falling rock. The only way to have no risk is to stay out of the mountains.

In terms of land use planning the choice is not between "no risk" and "risk" but between "acceptable risk" and "unacceptable risk". The approving authority is then faced with deciding what is an acceptable risk. In a given situation however what may be an acceptable risk to one person or group may be an unacceptable risk to others, and what may be an acceptable risk so long as it is only a future possibility may not be acceptable if it should become a present reality.

In planning a military operation a general may decide that the risk of 50% casualties is acceptable in terms of conduct of the war but this degree of risk may not be acceptable by his

troops. And if the casualties instead of being "possibly 50%" are in fact "actually 50%" this may not be acceptable to the political superiors of the general.

As discussed in previous sections, the risk of a catastrophic rockslide at any specific time and place is very small but the risk of a catastrophic rockslide at the site of a previous catastrophic rockslide is vastly greater than at a site where there has been no rockslide since the end of the ice age 10,000 years ago.

A catastrophic rockslide would probably be regarded as "unforseeable" if it occurs at a site where no catastrophic rockslide has occurred before. If it occurs at the site of a previous catastrophic rockslide it might be regarded as forseeable even though the statistical chance of it happening was very small in any pre-slide analysis.

#### RISK OF CATASTROPHIC SLIDE AT SUNSHINE VALLEY

Two catastrophic rockslides have occurred at the location of the Hope Slide during the past 10,000 years and it should be regarded as a slide-prone site when compared to slopes where no catastrophic rockslide has occurred. Features of incipient rockslides are noted in the reports by Golder Associates and by Mathews directly behind the headwall and at the northwest edge of the scarp of the Hope Slide. Rockslides from this part of the scarp would be directed to the northwest by the pile of debris from the 1965 Hope Slide.

The property of Sunshine Valley Developments Ltd. as shown in The Golder Associates report extends from a point apposite the scar of



the Hope Slide southeast along the valley bottom for approximately four miles.

In my opinion the approving officer of the Ministry of Highways should not approve any development involving permanently inhabited structures on any part of the valley bottom where the ground was deposited or disturbed by the Hope Slide. In my opinion the degree of risk of a catastrophic rockslide which would affect structures along the valley bottom southeast from the limits of the ground deposited by the Hope Slide is sufficiently low that the approving officer would be justified in approving development in the remaining part of the Sunshine Valley Subdivision subject to any constraints he may impose relative to the hazard of snowslides and rolling boulders.

#### HAZARDS FROM SNOWSLIDES AND ROLLING BOULDERS

The Golder Associates report (p.4) notes two problem areas aside from the Hope Slide which should be considered in developing the valley. These are a small avalanche track on the east side of Mount Potter and another above Sumallo Lodge. In addition in an area southeast of Tashme large boulders have fallen onto the valley floor from an area of very open jointing on the valley slope.

Neither the Golder Associates report nor the current overview should be regarded as a comprehensive evaluation of the localized hazard of snowslides or minor rockfalls relative to the proposed development.

A specific evaluation of the hazard from these sources should be made relative to the planned development of the subdivision.

## APPENDIX

- Golder Associates (1975) Report on a Geotechnical Investigation of Sunshine Valley, B.C. (report prepared for Sunshine Valley Developments Ltd., Vancouver, B.C.).
- Horcoff J. (1969) Comments Pertaining to the Report on a Geotechnical Investigation of Sunshine Valley, B.C. by Golder Associates. (Internal report Ministry of Highways of British Columbia).
- Mathews W.H. and K.C. McTaggart (1969) The Hope Landslide, British Columbia. Proceedings of the Geological Association of Canada. Volume 20 pp 65-75.
- Mathews W.H. (1977) Letter re Sunshine Valley report, Hope, B.C. addressed to Mr. John Kerr, B.C. Department of Highways dated February 21, 1977





**Golder Associates**  
CONSULTING GEOTECHNICAL ENGINEERS

REPORT ON A  
GEOTECHNICAL INVESTIGATION  
OF  
SUNSHINE VALLEY B.C.

Distribution:

- 6 copies - Sunshine Valley Developments Ltd.,  
Vancouver, British Columbia
- 2 copies - Golder Brawner & Associates Ltd.,  
Vancouver, British Columbia

February, 1975

V 74108

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### ABSTRACT

An investigation has been undertaken to assess the landslide hazard potential of a development in the valley southeast of the Hope landslide in British Columbia.

The valley slopes surrounding the development were covered on foot and then again from the vantage of a helicopter.

It is concluded that while the possibility exists of a further slide from the area just north of the 1965 slide, such failure would pose no threat to the development. The remainder of the valley walls appear secure from major instability.

Other risks in the valley from minor rock fall and snowslide are discussed with appropriate recommendations.

## INTRODUCTION

In April of 1974, Golder Brawner & Associates were retained by Mr. D.W. Low, of Sunshine Valley Developments Ltd. Mr. Low requested a general geotechnical investigation on the stability of the Sunshine Valley slopes about 12 miles southeast of Hope, British Columbia and an opinion on the likelihood of future, large-scale slope failures similar to the 1965 Hope Slide occurrence.

The field investigation was initiated in May and completed in September of 1974. During this period eight days were spent on site by Mr. Rippere, who was also accompanied by Mr. Brawner during a helicopter inspection. At the client's request, the investigation was completed on the southwest side of the valley prior to beginning work on the northeast slope.

The approach taken on the job was to traverse the valley slopes seeking an explanation for changes in vegetation patterns or density as well as observing visible rock conditions.

As all mountains are gradually eroding, the problem was not one of merely identifying landslide debris, but rather of detecting presently or potentially active portions of the hillside. Normal, multi-generational vegetation and dense mossy ground cover is possible only where rock conditions have been stable for hundreds of years. Vegetation will be immature or lacking on slopes that have moved more recently. Most commonly the causes of disturbed and abnormal vegetation are disease and fire. In fact, with the exception of the Hope slide, all anomalous vegetation patterns around Sunshine Valley appear to be attributable to fire, disease, snow slides, or the availability of water.

Figure 1 is a photo-mosaic showing the spatial relationship between Sunshine Valley (foreground) and the scar of the Hope landslide. Figure 2 shows the approximate property boundary of Sunshine Valley Developments Ltd. and Figure 3 shows the locations of the photographs taken around the property



which are then used to describe the geotechnical and stability conditions of the slopes surrounding the Sunshine Valley property.

A section describing the Hope slide through photographs follows. The report concludes with a discussion of the implications of the Hope slide, the probability of future major slides in the area, and its impact on development of the Sunshine Valley property.

We have discovered no evidence suggesting that the slopes above Sunshine Valley southeast of the Hope slide have an abnormal tendency to be eroded through major landslides and therefore development within this portion of the valley appears to be subject to no unusual degree of risk.

*Major landslides are not necessarily abnormal w/ a lot of irregular*

#### GEOTECHNICAL CONDITIONS ON THE VALLEY SLOPES

Sunshine Valley is a flat-bottomed, sediment filled trough averaging about 1/2 mile in width with mountain slopes rising 3,000 and 4,000 feet above the valley floor on both sides. Overall slopes on the northeast wall lie at about 33° while the southwest slopes average somewhat steeper, about 40°. This difference is the result primarily of the relationship between the valley geometry and the geometric framework of the pervasive geologic structure.

It is a universal condition of land surfaces that irregularities tend to be smoothed out. Thus all mountains are gradually being worn away by natural erosion, the debris from which accumulates at some lower elevation. When mass movement occurs, the word "landslide" is used to describe the process, and it is defined in the American Geological Institute Glossary of Geology (page 395) as follows:

landslide: A general term covering a wide variety of mass movement landforms and processes involving the moderately rapid to rapid (on the order of one foot per year or greater) downslope transport, by means of gravitational body stresses, of soil and rock material en masse. Usually, but not always, the displaced material moves over a relatively confined zone or surface of shear. The wide range in site variables, structure, and in the material properties affecting resistance to shear, as well as in the factors affecting the driving forces, results in a great range in landslide morphology, rates, and patterns of movement, and in scale. Landsliding is usually preceded, accompanied by, and followed by perceptible creep deformation, along the surface of sliding and/or within the slide mass. Terminology designating particular landslide types generally refers to the landform as well as the process responsible for the landform, e.g. rockfall, talus, translational slide, rockslide, block glide, debris slide, avalanche, earthflow, mudflow, quick clay slide, liquefaction slide, slump, rotational slide, etc.

There is very little mountain topography around the world today which does not demonstrate the scarring of landslide processes.

Figure 14 is an enlargement of the geologic map by McTaggart and Thompson (1967) showing the gross geologic aspects of the country surrounding Sunshine Valley. Figures 15 and 16 present the data developed during this investigation.

SPECIFIC CONSIDERATIONS FOR DEVELOPMENT OF SUNSHINE VALLEY  
- APART FROM THE HOPE SLIDE

During the investigation two problem areas were identified, ignoring the Hope slide for the moment, that need to be considered in developing the valley.

1. Snowslides

There is a small avalanche track on the eastern flank of Mt. Potter, LS-7, Sec 1 as shown in Figure 15, and another immediately behind and above what is now called Sumallo Lodge. Development should be kept away from the path of snow moving down these and other identifiable snowslide paths.

Should be  
be  
considered  
in  
development  
of  
the  
valley

Have these been identified?  
The 5th section of the report

2. Loose Boulders

In LS-3 and 4, Section 6 southeast of Tashme there is an area of very open jointing directly above the valley floor. This area is shown in Figure 17. Large boulders have rolled from this area in the recent past and we would recommend that permanent structures not be placed within 150 ft. of the toe of this slope.

might be  
large rock

h? (no structures)

If this location is otherwise very desirable, most of the problem potential from this area of loose rock can be alleviated through an adequate heavy scaling program.

soft soil  
by the

A further point is worth raising. LS-1, 7, 8 and 10, Section 14; LS-4, Section 13; and LS-13, 14 and parts of 11 and 12, Section 12 of the Sunshine Valley property cover much of the debris from the Hope slide itself. There are open cracks at the top of this slide and occasional small rock falls from the failure scarp even now, almost ten years after the major failure. We recommend that no permanently inhabited structures be developed on the ground deposited or seriously disturbed by the 1965 landslide. See Figure 2



for the general limits of the area involved. If it is decided to define a line beyond which development is to be so restricted, such a line should be staked out in the field and surveyed as it is not possible to identify precisely section corners and other benchmarks on the existing aerial photographs.

Otherwise it appears that the predominant hazard potential to residents of Sunshine Valley will derive from the normal risks of toppling trees, forest fires, floods, earthquakes and the like.

#### THE 1965 HOPE SLIDE AND ITS IMPLICATIONS

The most visible threat to development in Sunshine Valley lies in the implications of the bald scar (Figure 18) and the estimated 130,000,000 metric tons (Mathews and McTaggart, 1969, pg. 65) of rock debris filling the valley floor at the northwest end of the property. This is the remnant of the Hope slide of January 9, 1965, the details of which have been described by Mathews and McTaggart (1969). There is still discussion of the cause of this slope failure and as part of the present investigation the scarp was examined in detail to determine whether the conditions involved in the slide were unique to the slide area or whether they were pervasive along the northeast wall of the valley to the southeast.

Based on this investigation no evidence was observed that in our opinion suggests a major slide will occur southeast of the 1965 scar. Our interpretation is outlined in the following text.

Figures 19 and 20 are aerial photographs of the Hope slide area taken in 1973 and 1961 respectively. As the overlay to Figure 20 shows, the limits of the 1965 slide are almost wholly confined to the scar of an ancient landslide. The photos suggest that the only "new rock", as opposed to existing

slide debris, involved in the 1965 failure came from the headwall area of the earlier slide. It is possible that an investigation of this area prior to the 1965 slide and even before the ancient slide would have revealed extensive cracking of the sort shown in Figure 25.

Landslides, being the result of gravitational loads (vertical) which exceed the available effective strength resisting shear along the zone of slippage, invariably have a portion of the headwall scarp which is near vertical. This condition develops because at some point in the upper reaches of the developing slide the uppermost blocks become disconnected from the main slide mass.

Since small amounts of cohesion have a large effect on shear resistance for relatively small blocks, movement ceases in the upper blocks.

This has two effects which cause the headwall area of a slide of this type to be characteristically unstable:

1. The slope here is overly steep.
2. The remaining material has undergone some movement and is therefore partially broken, contains well developed tension cracks (as shown in Figure 25), and has only residual strength values remaining along the zone of shearing.

To speculate about the cause of the 1965 slide we will consider the following observations which have been developed from the aerial photographs, the field observations, and recorded by Mathews and McTaggart (1969).

1. As shown in Figure 19, there are several groundwater springs which flow from the central portion of the slide area.

2. Figure 20 reveals that an extensive debris pile covered the area of these springs (shown on Figure 19) prior to the 1965 slide. Figure 23 suggests the thickness of debris was in the order of 100 feet or greater, which is further documented in Mathews and McTaggart (1969, Figure 3, pg. 71).
3. Figure 20 reveals virtually all of the topographic characteristics in the lower half of the present slide scar, suggesting that rock "in situ" beneath the slide shown in the 1961 air photo is still in-place at this time.
4. Figure 21 suggests the hard, blocky aspect of the in-place rock around the Hope slide.
5. Figure 25 shows the character of tension cracks that probably existed in the headwall area prior to 1965 - angular blocks held together only by the keying action of the jointed material.
6. Air temperatures probably were not warmer than 32°F for twenty-five days prior to the slide (January 9). (Mathews and McTaggart, 1969, pg. 69)
7. Some snow had fallen on all but three of these days. (op. cit., pg. 69)
8. Two minor earthquakes (3.2 and 3.1 Richter magnitude) were recorded in the vicinity within the time frame of the landslide. (op. cit., pg. 69)

From these observations, we can hypothesize that the debris pile in the scarp of the ancient slide was insulated from the atmospheric temperature conditions. The springs would continue to contribute water to the porous debris beneath the surface blanket and some of this material may have



been near saturation at the time of the slide.

Either of the two earthquakes might have shaken the cracked, blocky headwall area enough to unkey strategic portions, permitting the mass to drop onto the wet debris setting in the central portion of the ancient slide area. As this weight came onto the debris, high water pressures would be generated throughout saturated portions of this material, causing liquefaction and precipitation of the mass onto the valley floor. This impact, in turn, liquefied the saturated fine sediments around Outram Lake which splashed along the valley trench in both directions.

In its important aspects, the above discussion parallels Mathews and McTaggart's (1969) impressions of the 1965 slide.

What is important at this time, however, is consideration of the likelihood of a repeat situation and/or a similar slide developing to the southeast. If it is important to the development of the Sunshine Valley property to debate the cause of the Hope slide, then it is the ancient slide with which we must be concerned because the 1965 event was a consequence of the earlier failure. We believe the prime cause of this earlier slide may have been the apparent thin veneer of altered sediments and volcanics overlying the quartz-diorite intrusive and felsite sheets in this portion of Johnson Peak, as shown in McTaggart and Thompson's map, Figure 14. While we recognize that this mapping was done on a broad scale, the suggestion of the shape of the intrusive and the prominence of the felsite sills in the presently exposed failure surface lend some weight to this argument.

To the southeast of the 1965 slide, the intrusive apparently turns deeper into the hillside causing an increasingly thick and therefore more stable capping of sediments and volcanics (again from McTaggart and Thompson's mapping).

During the field investigation no major difference was observed in the structural fabric of the rock or groundwater conditions in the slide area and those further southeast. There are subtle indications, however, that the valley wall may be a few degrees steeper on the average in the vicinity of the slide than it is to the southeast or that there may be a slight change in the strike of the regional foliation. There is also a weak rock type in the slide debris which was not observed on the hillside to the southeast. This rock is a serpentine-chlorite schist, some portions of which have a high talc content. Although an attempt was made to identify felsite dykes and sills in this area to the southeast, none were observed. These three conditions would contribute to the lowering of the factor of safety of the overall slope in the slide area or, conversely result in a higher safety factor for the slopes to the southeast.

In any event, it is the Hope slide area itself that has been historically unstable, while the valley walls to the southeast, overlooking most of the Sunshine Valley property have shown no indication of susceptibility to major landslides.

During this investigation the valley wall southeast of the 1965 slide has been explored in detail, both on foot and by helicopter, and no evidence, such as tension cracks, erratic alignment of tree trunks, sequential minor rock slides, etc., suggesting future major instability was observed.

What about the possibility of continued headwall activity in the slide area and what would the consequences be?

As Figure 19 shows, most of the mountain above the ancient slide was removed in the 1965 event. Additionally one would expect that most of the cracked and weakened rock in the headwall would have collapsed at the time of failure. The tension crack system shown in Figure 25 involves about 120,000

cu. meters or about 2 tenths of one per cent of all the material in the 1965 failure. If this rock should collapse, in all probability little of it would reach the valley floor. We recommend that this crack system be brought to the attention of the British Columbia Department of Highways, since if the rock does fail some blocks could reach and endanger the highway. It appears practical to bring this rock down while traffic is temporarily kept out of the potential slide area.

In terms of the area disturbed in 1965, most of the damage caused in the valley floor in 1965 resulted from the impact of the slide material into the saturated valley sediments and their subsequent liquefaction. As shown in Figures 1 and 13, there is now as a result of the 1965 failure, a permeable coarse-grained capping in excess of 100 feet in thickness overlying whatever silty material may still underlie this debris. This would act as a shock absorber and consequently reduce any liquefaction effect that would develop in a repeat situation.

Therefore, from geological interpretation, we do not believe that the conditions present in the immediate vicinity of the slide scar at higher elevations on Johnson Peak and to the southeast are such that a repetition of the 1965 event should be anticipated.

The possibility of additional major slope instability certainly exists to the northwest of the slide scar but if this portion of Johnson Peak should collapse, we believe the effect would be isolated from Sunshine Valley by the debris pile from the 1965 slide.

Figures 22 and 23 illustrate evidence of pre-1965 landslide activity in this area, and Figures 24-26 show the evidence of future instability in the slide area. In addition, Mathews (1969, pg. 68) mentions an area of marginal stability below the 3700 elevation along the north edge of the present scarp.



The preceding portions of this report have been reviewed by and discussed with Dr. W. Mathews, Professor of Geology at U.B.C.

#### COMPARISON OF LANDSLIDE RISK TO OTHER NATURAL HAZARDS

All structures are vulnerable to natural hazards such as floods, fires, and earthquakes. It is instructive to estimate the likelihood of a major landslide destroying portions of the proposed development and compare that likelihood with the risk level accepted by inhabitants of areas subject to severe earthquakes or floods.

Since there is no evidence of major landsliding along the northeast valley slope south of the 1965 slide area we cannot estimate a statistical probability of future landslides in that area. We can however estimate the probability of recurring instability in the immediate area of the Hope slide itself. In the following paragraphs we show that even this level of danger is minimal compared to the threat of a devastating flood or earthquake accepted by residents of the Fraser Valley.

#### PROBABILITY OF FUTURE MAJOR SLIDES

Another approach to the problem of determining the likelihood of future slides is through the use of statistical procedures.

A major landslide occurred in 1965, and another  $9,680 \pm 320$  years ago (Mathews, 1972). The accepted date for the ice retreat from this area is just over 11,000 years ago. So at least two major failures have occurred within the last 10,000 years. To provide conservative statistics we have assumed there were three major slides in the past 10,000 years in this area. Assuming that the probability of a major slide occurring in any given year is constant, we calculate the most likely probability of additional major slides from the immediate

area of the slide scar as follows.

procedures).

In 100 years the probability of another major failure is 3.0%  
 In 500 years the probability of another major failure is 13.9%  
 In 1,000 years the probability of another major failure is 29.5%

To put this in perspective, engineered structures are designed to withstand certain load conditions from natural events. For instance, we speak of a 100-year wind, or a 50-year snow load, a 100-year earthquake, or a 100-year flood. Internationally a design life of 50 years is used for buildings and highways, and 100 years for water storage and hydro dams. That is to say that a building is designed to withstand the load conditions resulting from the greatest magnitude earthquake anticipated in a fifty year period. The 100-year earthquake has a larger magnitude and may exceed the design strength of the structure.

Typical design criteria in the Vancouver area are as follows:

100-year wind	100 mph
100-year earthquake	7 - 7.5 Richter magnitude

A magnitude 7 earthquake in Vancouver would cause severe damage.

According to Sheldon Cherry (UBC Dept. of Civil Engineering, personal communication) the 100-year earthquakes and resulting damage in representative cities is as follows:

<u>City</u>	<u>Richter Magnitude</u>	<u>Damage</u>
Vancouver	7.0 - 7.5	Severe
Victoria	7.0 - 7.5	Severe
Seattle	7.0 - 8.0	Very severe
San Francisco	7.5 - 8.0	Extremely severe

The probability of a major flood causing "extensive-damage" in the Fraser Valley is 0.5 per cent in any one year, 39 per cent in 100 years.

(Dennis Russell - UBC Dept. of Civil Engineering, personal communication)

Thus, the 3 per cent probability of a further slide in the immediate area of the 1965 failure in 100 years is within the risk of natural calamity commonly accepted.

MINIMIZING THE HAZARD

There is an aspect of major landslides that can be utilized to provide advance warning of instability and therefore minimize the danger to life. As mentioned earlier, landslides are always preceded by a period of creep deformation during which a network of tension cracks appears in the developing headwall area. An annual helicopter reconnaissance of the mountain slopes above Sunshine Valley should reveal any developing cracks well in advance of a slide. Should such cracks be observed, more frequent observation and simple displacement monitoring instrumentation would provide adequate warning to residents of the valley.

CONCLUSION

Therefore we conclude from geological considerations that another major slide in the immediate area of the 1965 slide (except to the north and west) is very unlikely and to the southeast of the slide area is even less likely. Statistically the risk is within acceptable limits of international engineering design standards and whatever human risk does exist can be minimized through an inexpensive regular observation of the mountain slopes. It is our opinion that development of the Sunshine Valley property is reasonable provided that the problem areas mentioned in the section on Specific Considerations for Development of Sunshine Valley are avoided.


We further are of the opinion that there is no more and probably less risk of a major landslide in the southeast area of Sunshine Valley than the probability of a major earthquake in Victoria, Vancouver or San Francisco or a major flood of the Fraser River.



Yours very truly,

GOLDER BRAWNER & ASSOCIATES LTD.

Per:

  
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B.A., 1965, The University of British Columbia

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1. Gary, McAfee, and Wolf, 1972, Glossary of Geology: American Geological Institute, Washington, D.C.
2. Mathews, W.H. and McTaggart, K.C., 1969, The Hope Landslide, British Columbia: Geological Association of Canada Proceedings, February 1969, Volume 20, pages 65 - 75.
3. Mathews, W.H., 1972, Hope Landslide; 24th International Geological Congress, Guidebook for Field Excursion A05-C05.
4. McTaggart, K.C. and Thompson, R.M., 1967, Geology of Part of the Northern Cascades in Southern British Columbia; Canadian Journal of Earth Sciences, Volume 4, pages 1199 - 1228.
5. Nasmith, H., 1972, The Hope Landslide; 24th International Geological Congress, Guidebook for Field Excursion A08-C08.

4. If there were three slides in the last 10,000 years ( $s=3$ ,  $y=10,000$ ), then from Equation 3:

$$3 = 10,000 p - 200\sqrt{p}$$

$$\text{so } p = .0009$$

That is, there is a 95 per cent probability that the true value of  $p$  is less than .0009 (the most likely value is  $p = .0003$ ).

5. The probability of no slides in a  $n$ -year period is  $e^{-np}$ ; so using the most likely value of  $p = .0003$  we find:

Most Likely Probability

<u>Period (years)</u>	<u>Probability of No Slides</u>	<u>Probability of One or More Slides</u>
100	.9704	.0296
500	.8607	.1393
1,000	.7408	.2592

or using the conservative value  $p = .0009$ :

Worst Likely Probabilities

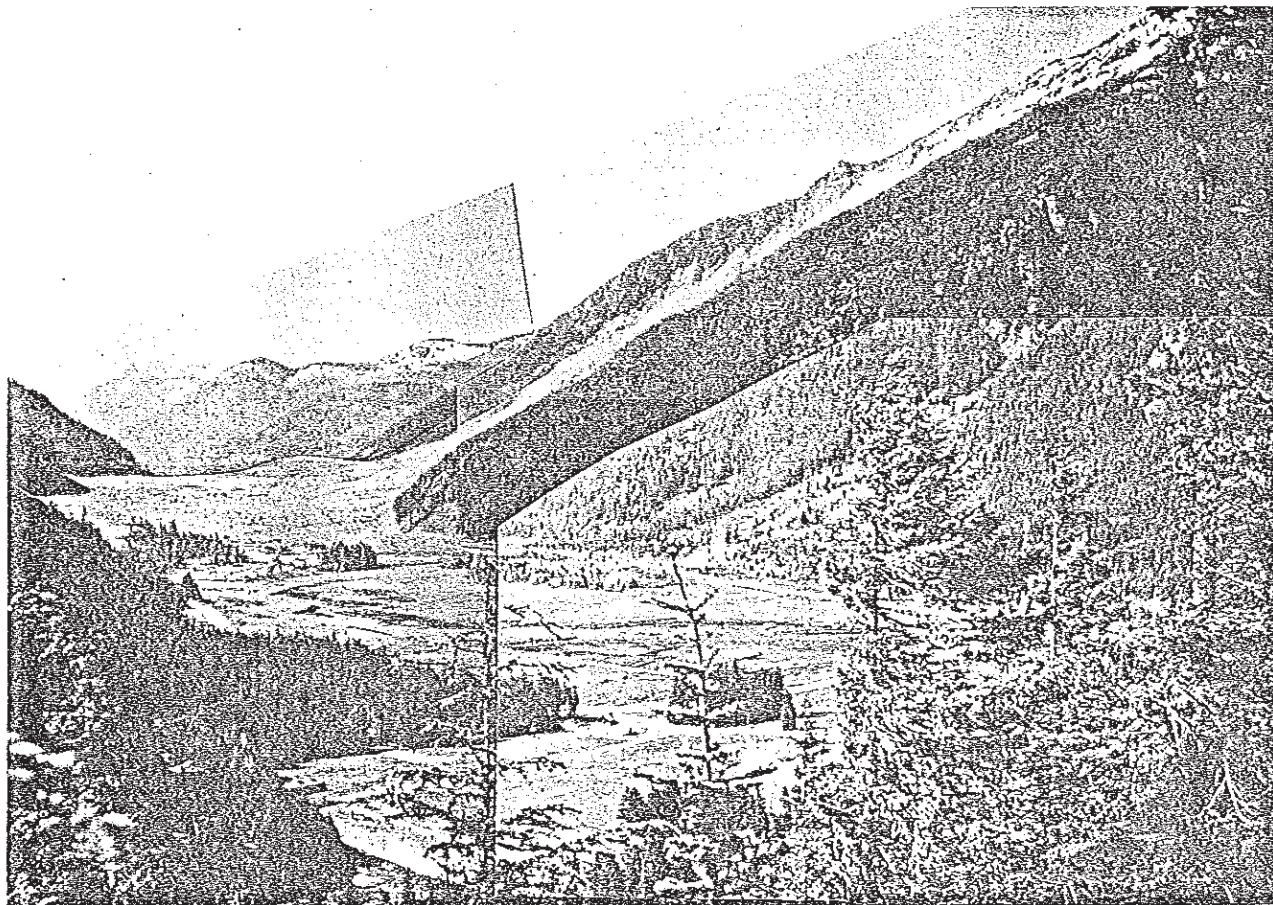
<u>Period (years)</u>	<u>Probability of No Slides</u>	<u>Probability of One or More Slides</u>
100	.9139	.0861
500	.6376	.3624
1,000	.4066	.5934

Ref.: Statistical Theory and Methodology in Science and Engineering. K.A. Brownlee John Wiley & Sons, 1960.



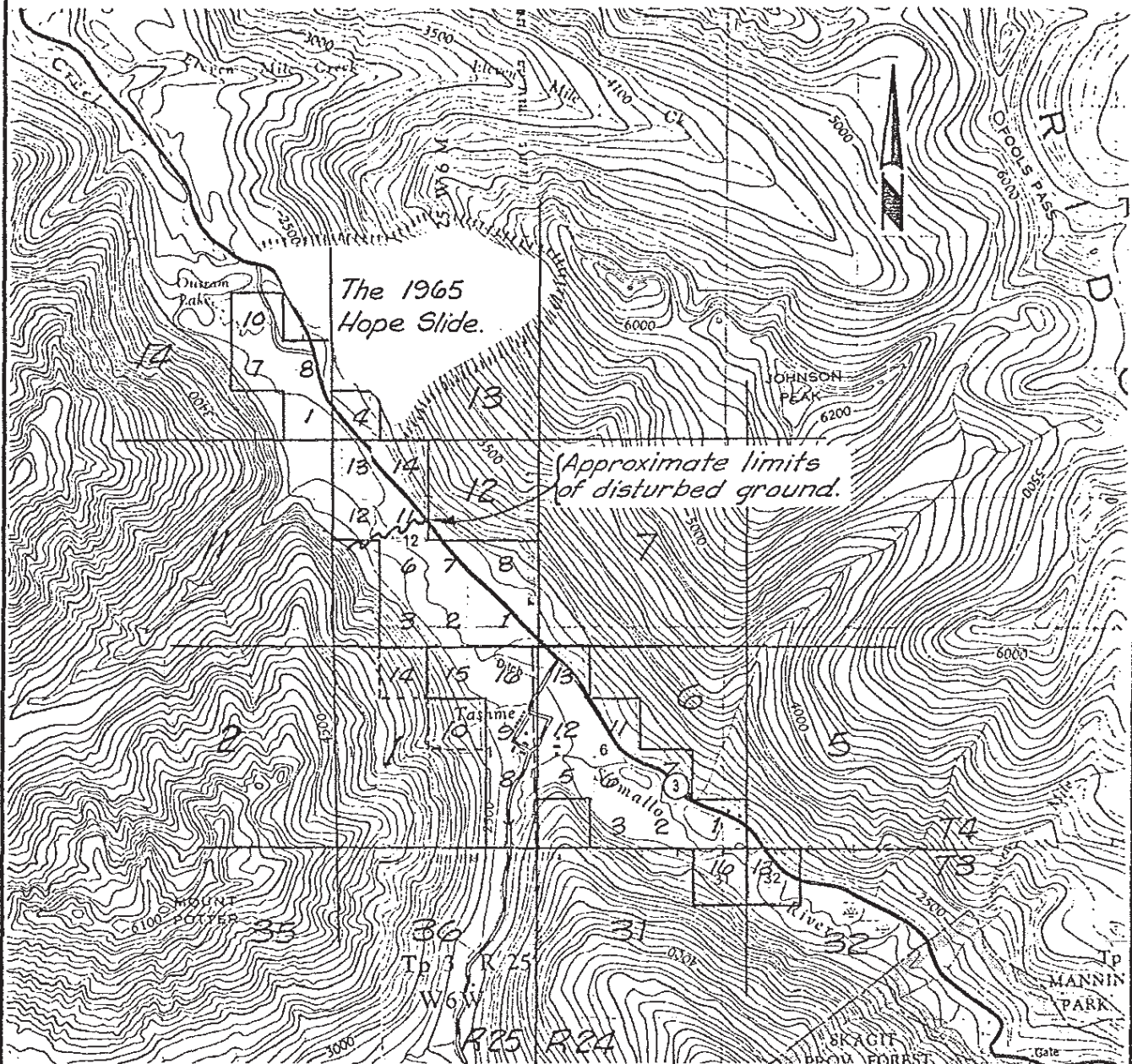
SUNSHINE VALLEY AND THE HOPE SLIDE

FIGURE 1



# SUNSHINE VALLEY PROPERTY LIMITS

FIGURE 2



Source: Sunshine Valley Developments Ltd.  
drawing No. LS-2, Date 17 July, 1970.

**Golder Associates**

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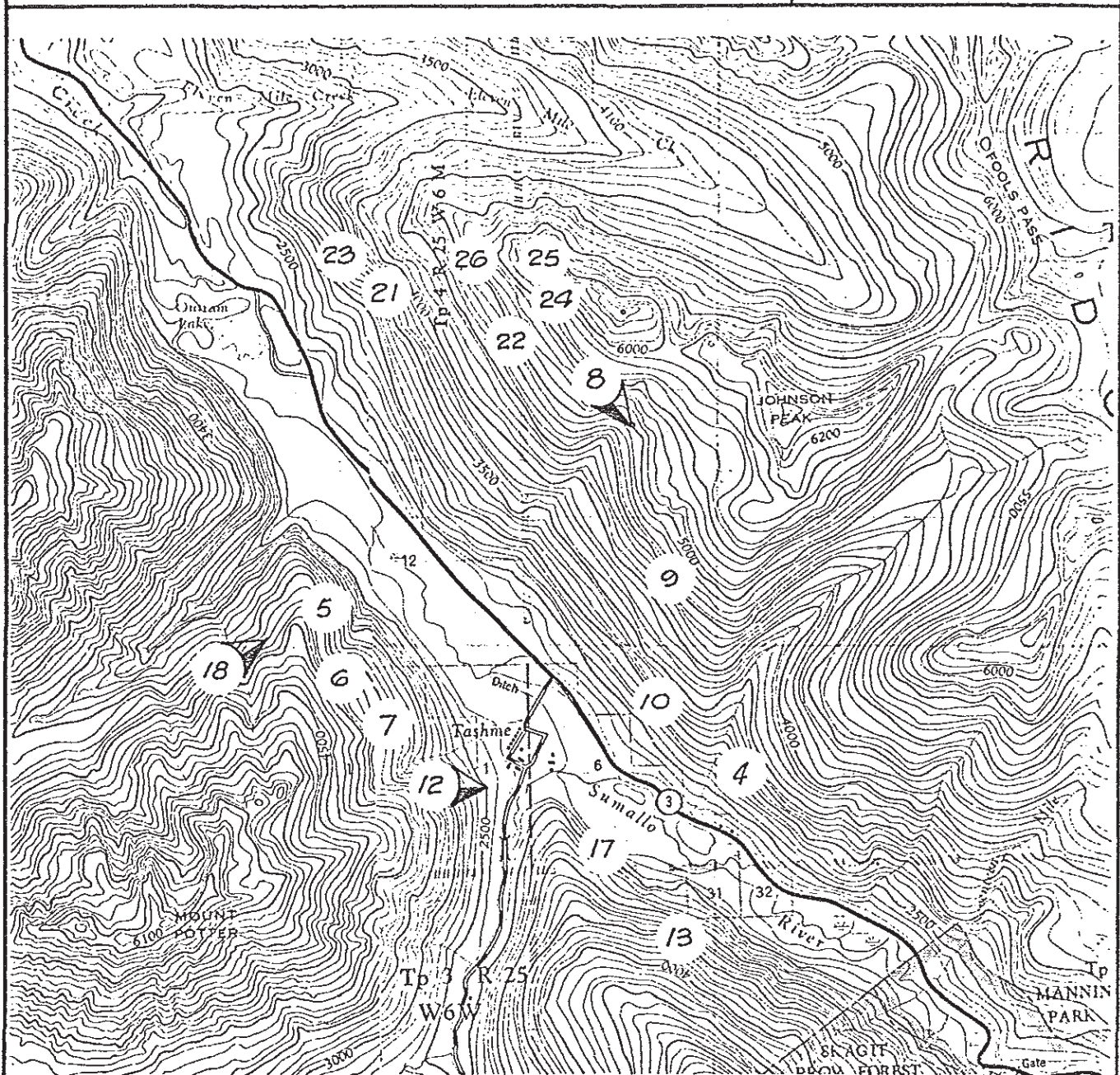
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Date Dec. /74

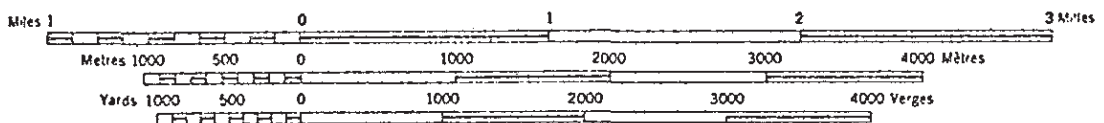


LOCATIONS WHERE PHOTOGRAPHS  
WERE TAKEN  
AROUND SUNSHINE VALLEY

FIGURE 3



SCALE 1:50,000 ÉCHELLE



CONTOUR INTERVAL 100 FEET  
Elevations in Feet above Mean Sea Level  
North American Datum 1927  
Transverse Mercator Projection

ÉQUIDISTANCE DES COURBES 100 PIEDS  
Élévations en pieds au dessus du niveau moyen de la mer  
Niveau de référence nord américain 1927  
Projection transverse de Mercator

*Numbers refer to figure number in this report.*

**Golder Associates**

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Approved VR

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TRA-2011-00209 Date Dec/74



## Examples of Mountain Erosion

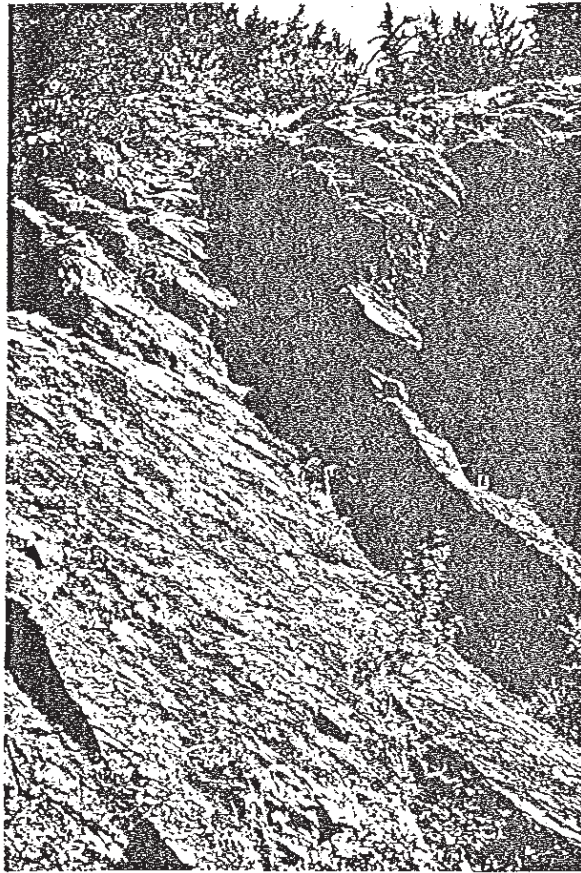


Figure 4

One of the most common causes of rock fall is the intersection of two weakness planes, here a combination of foliation and jointing, that is daylighted on the slope. The block of rock bounded by these surfaces tends to slide down the intersection and fall to the ground below.

Here, a small wedge failure directly up-slope from Sumallo Lodge is shown. Foliation is represented by the family of planes in the sunlight. The vertical joints forming the other half of the wedge are in shadow. A number of rock failures of this scale occur above the 3,500 ft. level along the slopes on both sides of Sunshine Valley.

Photo taken July 30, 1974.



Figure 5

Ancient landslide debris at about 3,500 ft. elevation along the southwest wall of Sunshine Valley. Note the angular, disordered rock debris supporting several generations of conifers and a dense covering of mosses and other ground vegetation.

Photo taken May 17, 1974.



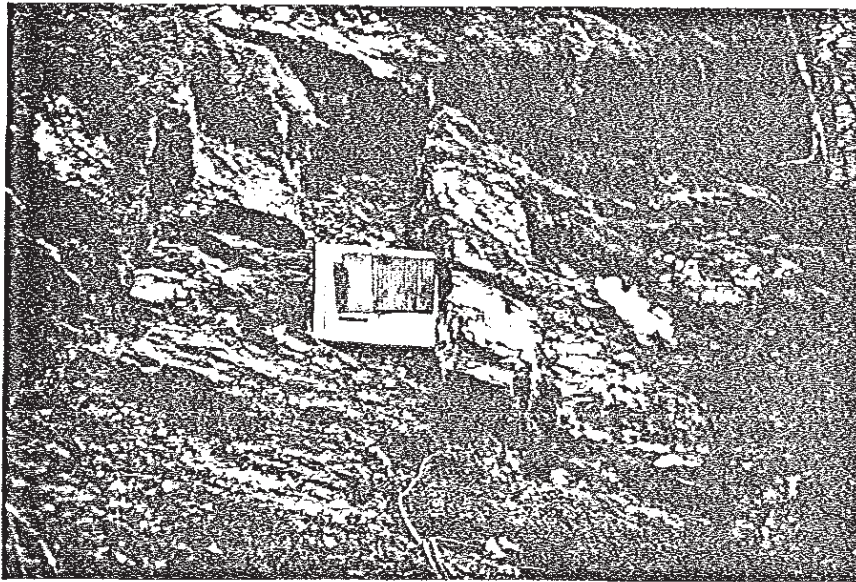


Figure 6

In-place pelite at the 3,700 ft. elevation along the southwest wall of the valley. The foliation, or bedding direction, dips to the southwest, away from the viewer. It is represented by the shaded surfaces in the photograph. The other two families of planes are joints.

As cycles of freezing and thawing loosen blocks within this structural framework, they fall free forming talus debris at the foot of the particular slope and leaving small cliff like rock faces standing at 60° or greater. The dense moss growth indicates slowness of this wasting process.

Photo taken May 17, 1974.

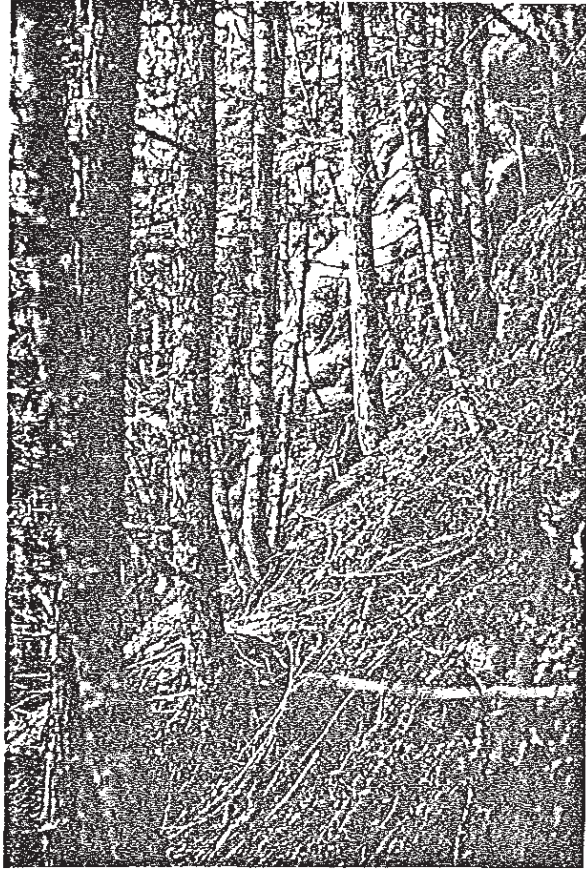


Figure 7

A 60° natural slope in pelite at the 3,500 ft. level along the southwest slope of the valley. The dense vegetation attests to the stability of the slope although it does form some talus as individual blocks are loosened by the cycles of freezing and thawing.

Photo taken May 17, 1974.

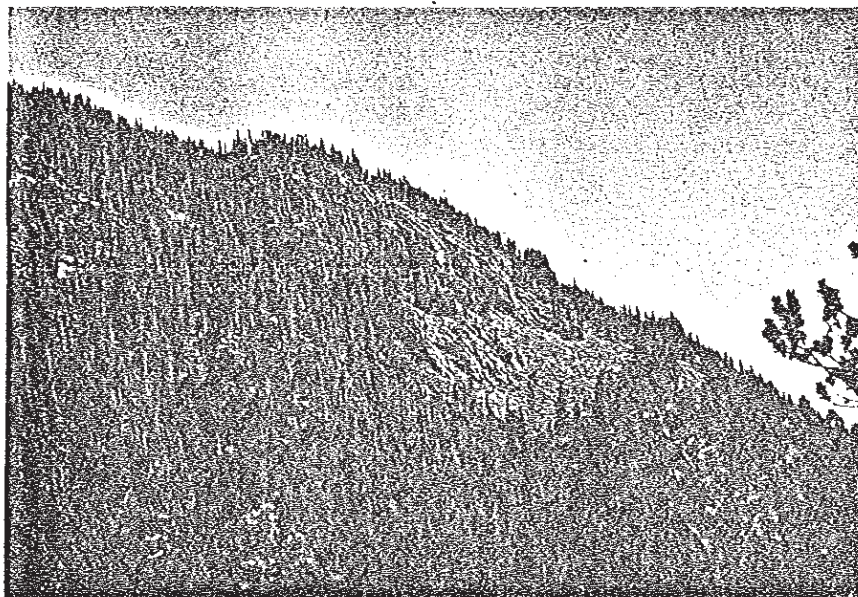


Figure 8

An overall view of the hillside structure and resulting talus at about the 5,500 foot level of Johnson Peak. The prominent down-slope structural fabric is easily seen in this photograph.

Photo taken September 20, 1974.





Figure 9

Foliation dipping parallel to the slope at the 4,500 ft. level of Johnson Peak. This structure is typical of the rock forming the northeast wall of the valley. The two vertical joint sets admit water to the near-surface portions of the rock mass. When the water freezes it forces individual blocks and plates loose which ultimately slip off down the slope, collecting in talus piles below.

Photo taken May 30, 1974.



Figure 10

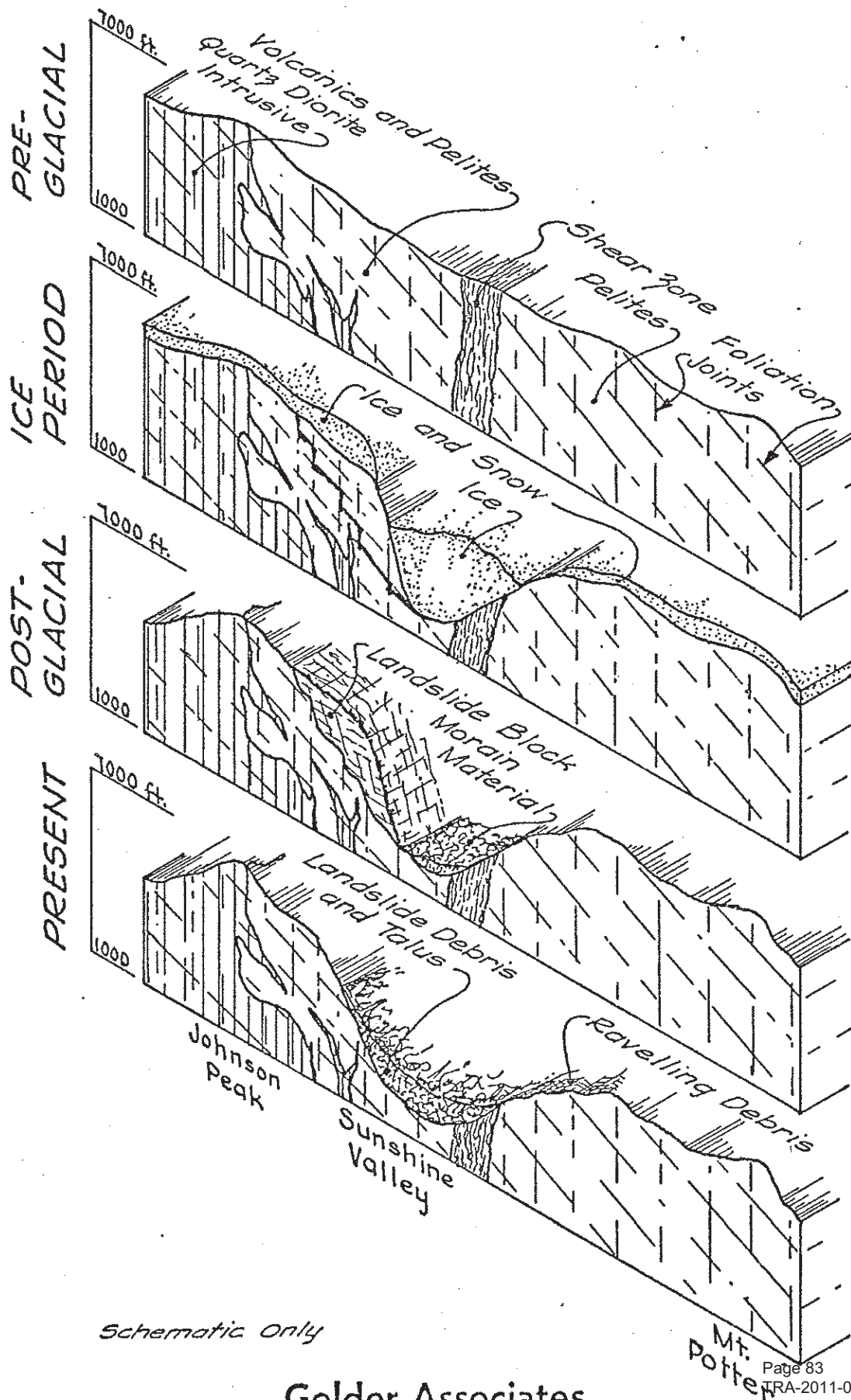
A 25° slope at the 3,000 ft. level on the northeast side of the valley. This slope is entirely talus and is actively migrating down-slope in a method comparable to soil creep, as evidenced by the curvature at the base of the tree trunks. The dense growth of ground level vegetation attests to the slowness of this creeping action.

The slope is rubbly underfoot and occasionally one can hear water flowing along the underlying rock surface.

Photo taken May 30, 1974.

# GEOMORPHOLOGIC EVOLUTION OF SUNSHINE VALLEY

FIGURE 11



Schematic Only

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Potter

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## Geomorphology of Sunshine Valley



Figure 12

The Sumallo River flowing southeast towards the Skagit Valley. The meandering of the river is evidence of the gentle gradient of the valley floor to the southeast. The sediments forming the valley floor are primarily alluvial and colluvial with a suggestion of some lake bed influence. It is inferred that at least one major landslide from the immediate area of the 1965 Hope slide blocked the valley in ages past interrupting normal drainage and ponding a lake behind it to the south. Sediments washing into the valley from the Sumallo River and other creeks and talus and slide debris from the surrounding hillsides gradually filled the valley until the water level was raised sufficiently to reverse flow, breaking through finally into the Skagit River to the south.

Photo taken May 21, 1974.

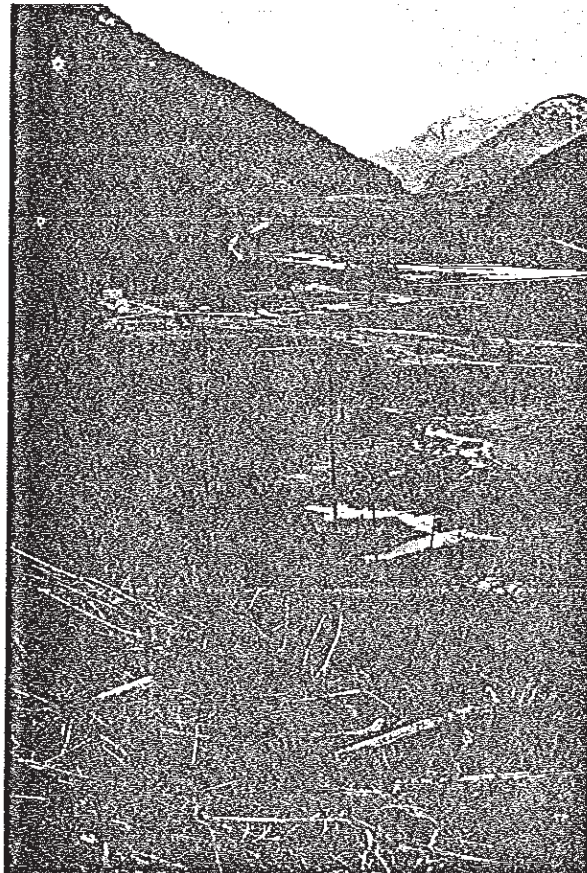


Figure 13

The upper portion of the debris pile from the landslide in LS-15, Sec 31. A forest fire is presumed to be the cause of the denuded vegetation in this area rather than instability of the existing slide scarp. Very ancient trees, up to several feet in diameter, are growing from the slide debris at the toe of this slope, suggesting that the slide area has been inactive for several hundred years.

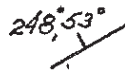
Debris from the Hope slide of 1965 forms the valley blockage in the distance.

Photo taken May 22, 1974.

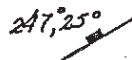


KEY TO THE GEOLOGIC MAPS

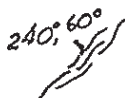
Figures 14, 15 and 16



Foliation, bedding (Strike, Dip)



Joints (Strike, Dip)



Faults (Strike, Dip)



Quartz Diorite



Basic Volcanic Rocks (Greenstones)



Pelite, Chert, minor Limestone



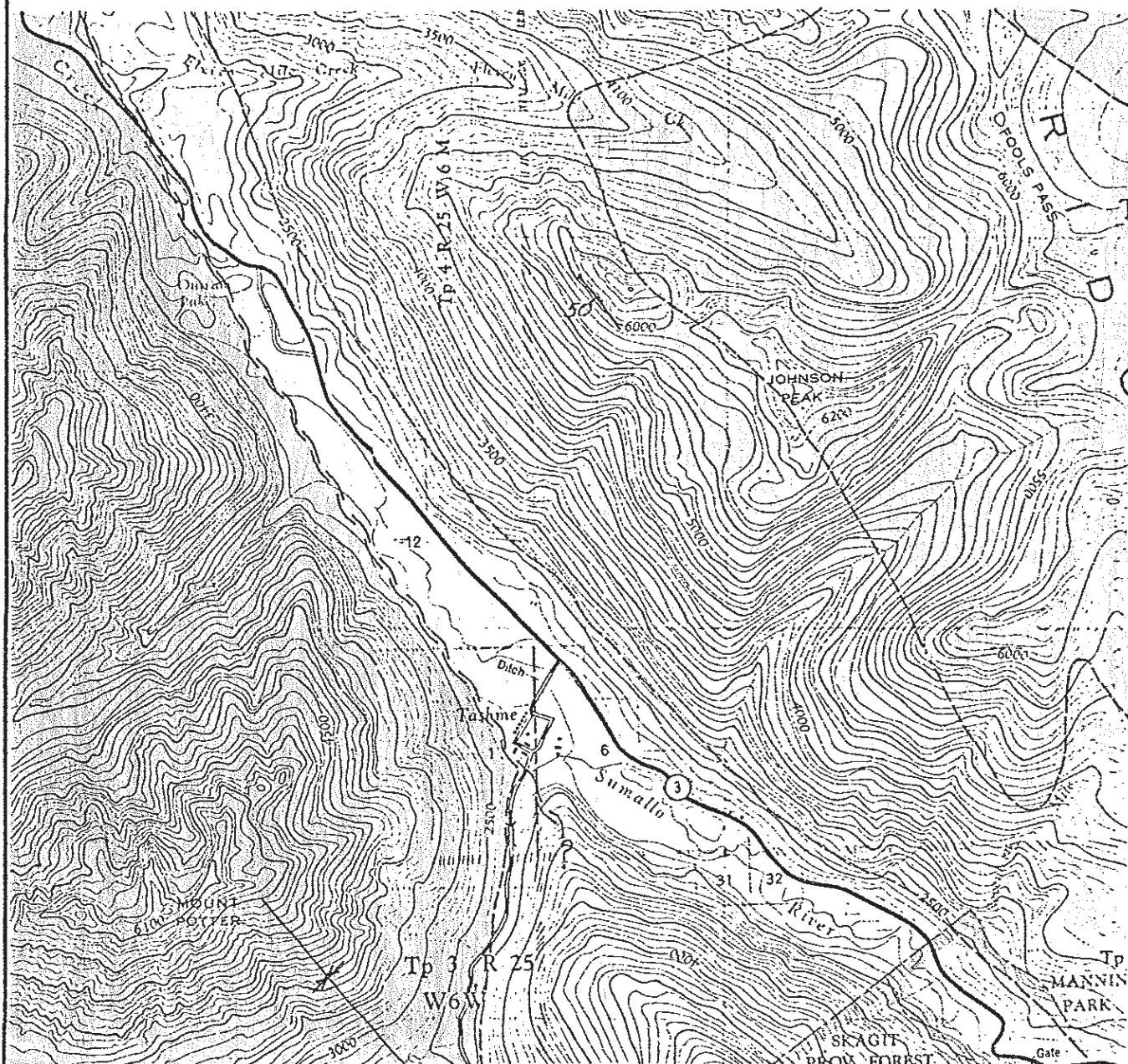
Chert, Pelite



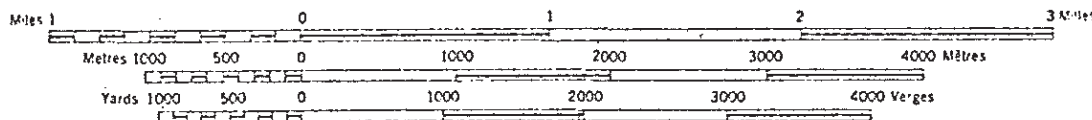
Limestone

LARGE SCALE GEOLOGY  
OF THE SUNSHINE VALLEY AREA

FIGURE 14



SCALE 1:50,000 ÉCHELLE



CONTOUR INTERVAL 100 FEET  
Elevations in Feet above Mean Sea Level  
North American Datum 1927  
Transverse Mercator Projection

ÉQUIDISTANCE DES COURBES 100 PIEDS  
Élévations en pieds au dessus du niveau moyen de la mer  
Niveau de référence nord américain 1927  
Projection transverse de Mercator

Geology from:

McTaggart and Thompson, 1967.

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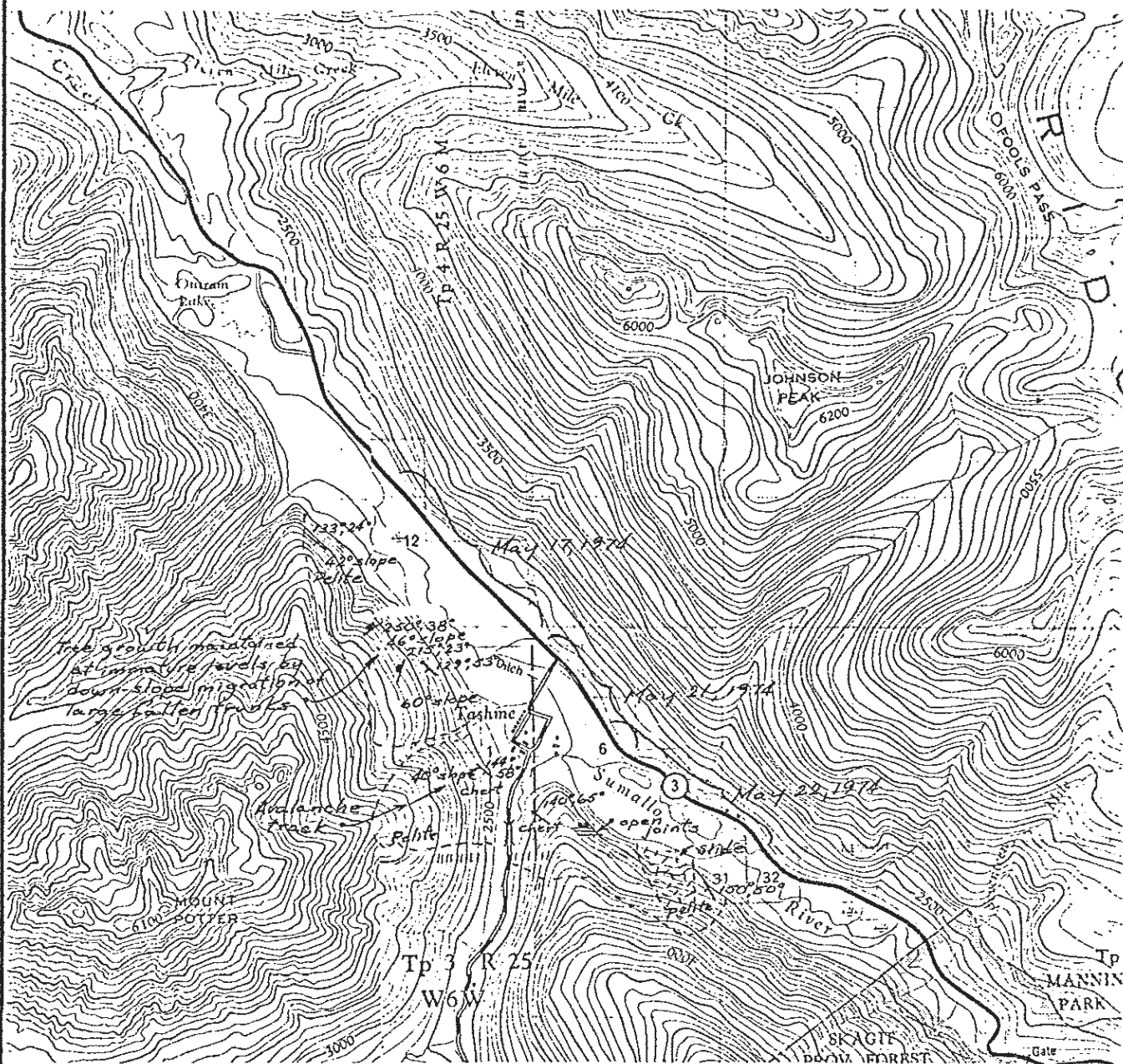
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Date Dec 74



# FIELD GEOLOGY OF THE SOUTHWEST SIDE OF SUNSHINE VALLEY

FIGURE 15



SCALE 1:50,000 ÉCHELLE



CONTOUR INTERVAL 100 FEET  
Elevations in Feet above Mean Sea Level  
North American Datum 1927  
Transverse Mercator Projection

ÉQUIDISTANCE DES COURBES 100 PIEDS  
Élévations en pieds au dessus du niveau moyen de la mer  
Niveau de référence nord américain 1927  
Projection transverse de Mercator

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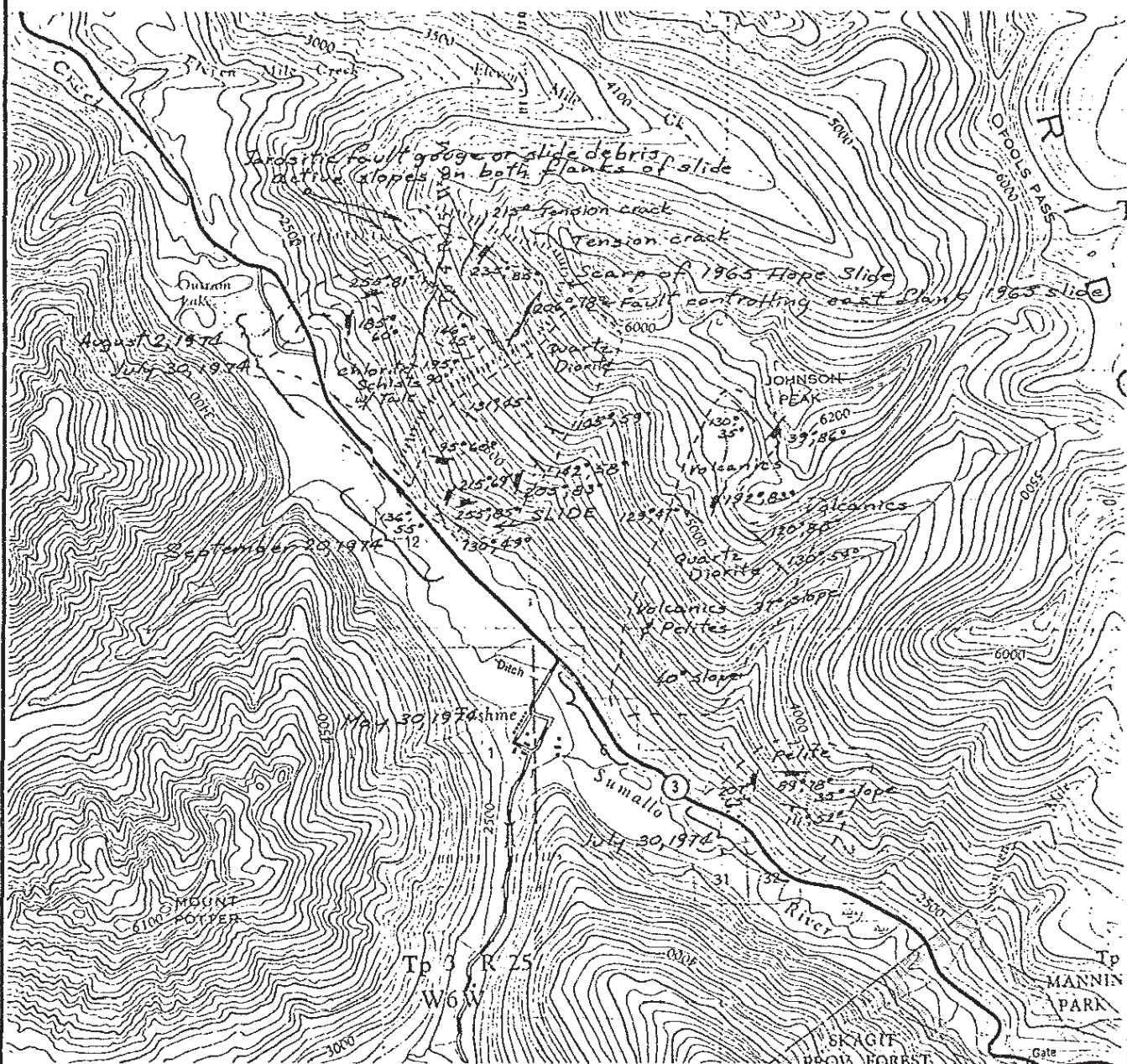
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Date *Dec '74*

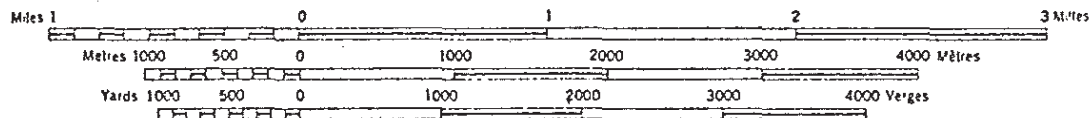


# FIELD GEOLOGY OF THE NORTHEAST SIDE OF SUNSHINE VALLEY

FIGURE 16



SCALE 1:50,000 ÉCHELLE



CONTOUR INTERVAL 100 FEET  
Elevations in Feet above Mean Sea Level  
North American Datum 1927  
Transverse Mercator Projection

ÉQUIDISTANCE DES COURBES 100 PIEDS  
Élévations en pieds au dessus du niveau moyen de la mer  
Niveau de référence nord américain 1927  
Projection transverse de Mercator

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Date *Dec '74*

### Particular Features in Sunshine Valley

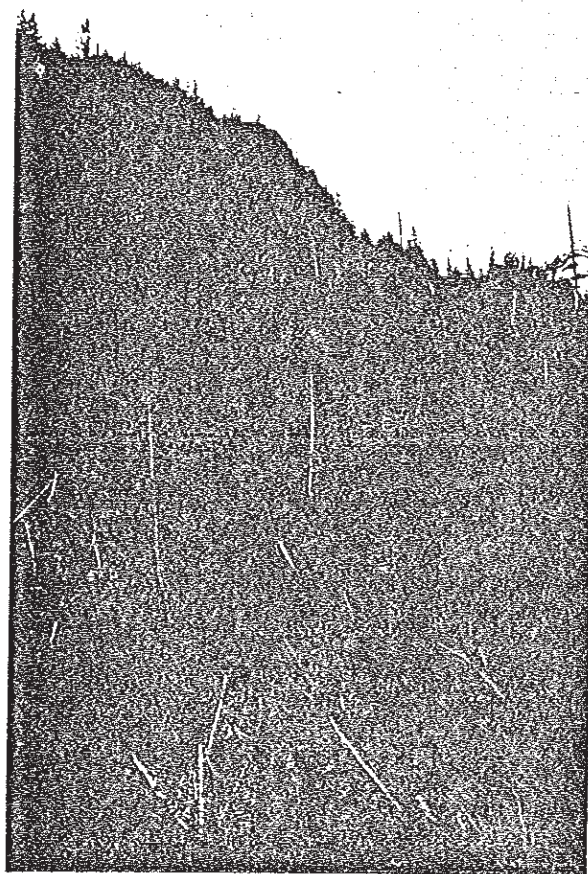


Figure 17

Open vertical joints in pelite at the 2,500 ft. level along the southwest side of the valley. The talus slope at the foot of the face is clearly evident in the photograph. Some of the debris from this rock slope consists of large boulders up to 10 yds.<sup>3</sup> in size which have rolled a distance up to 100 ft. out onto the valley floor.

Photo taken May 22, 1974.



## The Hope Slide

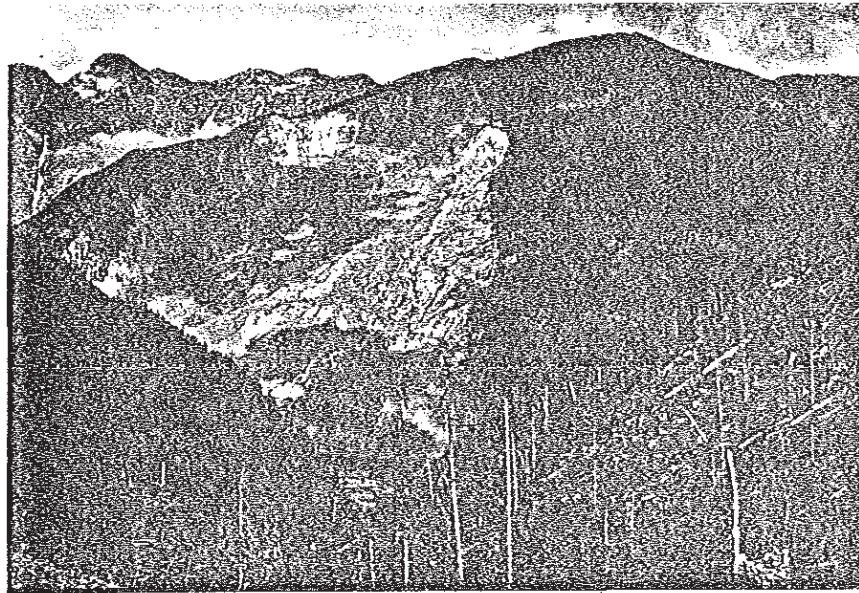


Figure 18

An overview of the scar from the 1965 Hope slide illustrating the magnitude of the failure with respect to the host mountain side.

The cloud shadows help define a diagonal line across the scar from lower left to upper right. The 130,000,000 metric tons of material that moved came almost entirely from that portion of the scar above the diagonal, sliding to the left. The flank of the hill below the diagonal has been denuded of soil and vegetation by rock bounding down hill from the failure above.

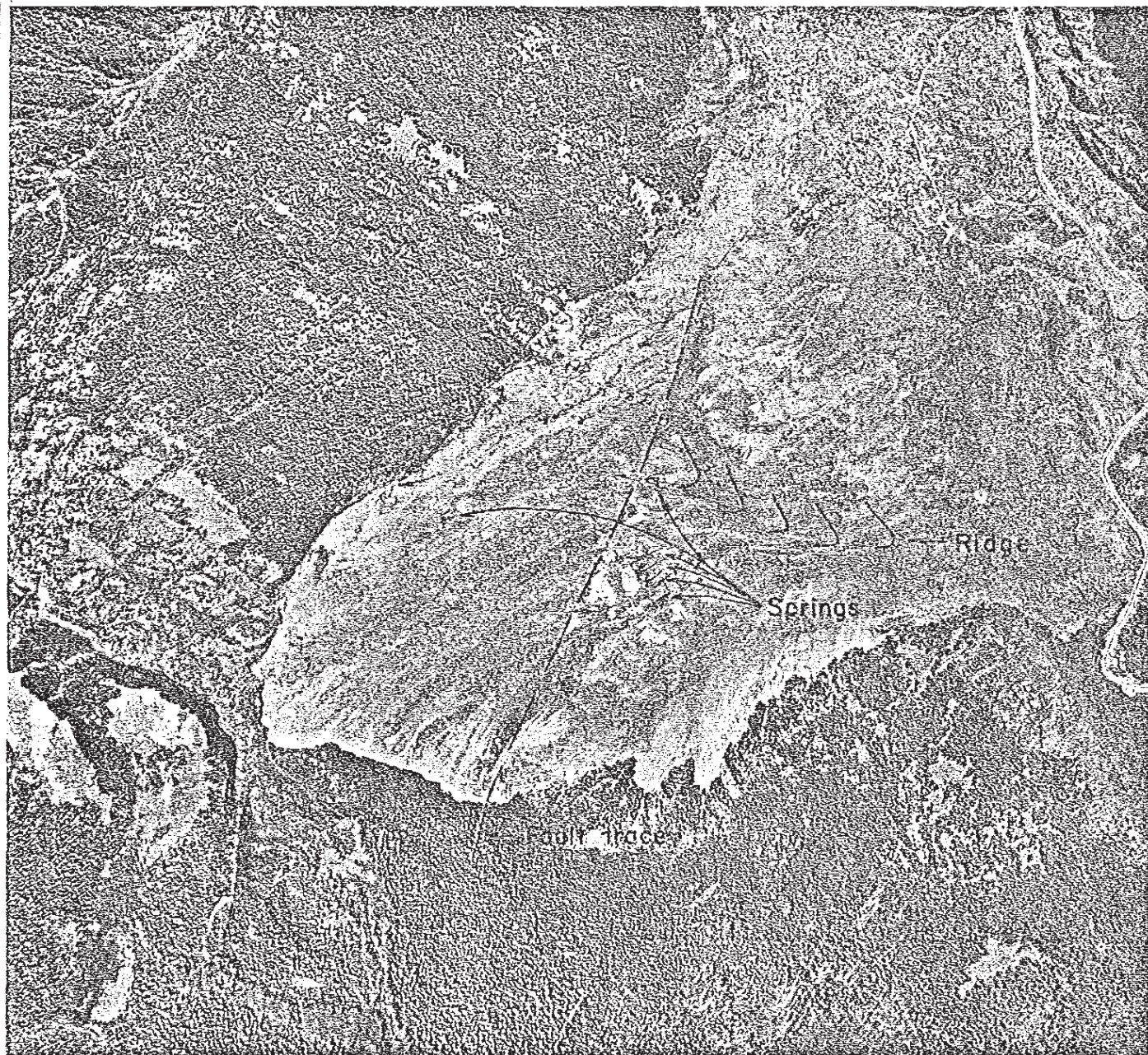


THE HOPE LANDSLIDE AREA  
1973

FIGURE 19

Project No. V-74/08

SOUTH



BC 7476 No. 101  
JULY 14, 1973

0 1/4 1/2 3/4 1 Mile

Approximate Scale

**Golder Associates**

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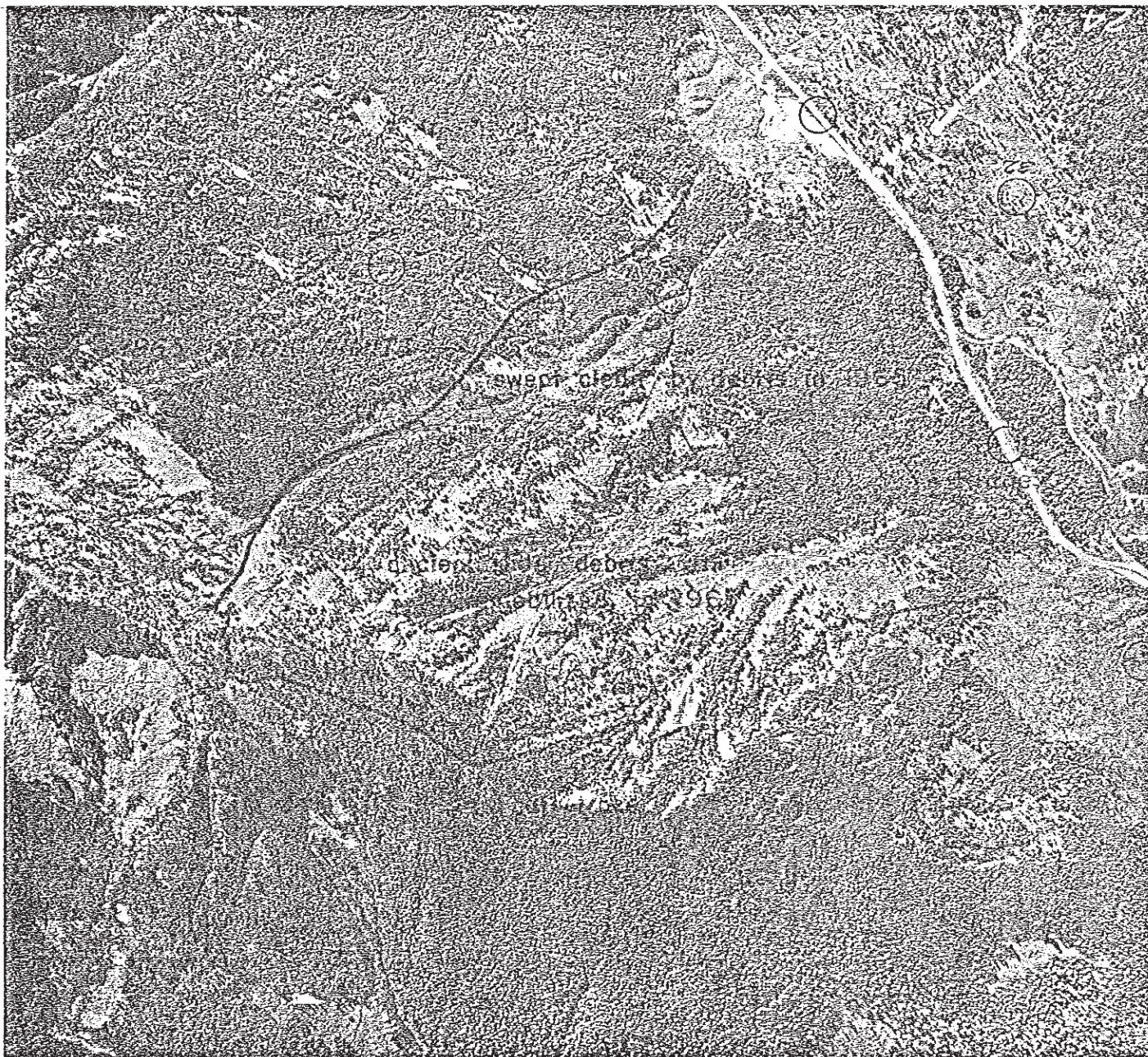


# THE HOPE LANDSLIDE AREA 1961

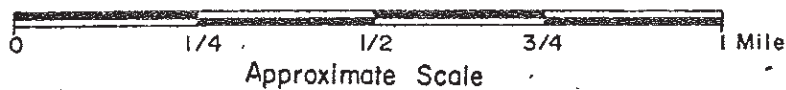
FIGURE 20

Project No. V-74108

SOUTH



BC 4014 : 24  
AUGUST 19, 1961



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Date *Dec 74*  
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## **The Hope Slide**

### **Structural Features Analogous to the Surrounding Valley**





Figure 21

The upper portion of the west flank of the Hope slide is shown in this picture. Note the blocky aspect of the rock exposed. This results from the two perpendicular vertically dipping sets of joints acting in concert with parting along the regional foliation, which dips toward the observer at about  $35^{\circ}$ . This is the same combination of structural elements that is shown in Figures 4, 6, 8 and 9.

Most of what remains of the overgrown part of the ancient slide surface is also shown in this photo as the tree covered area.



The Hope Slide  
Evidence of Earlier Major Slide(s)



Figure 22

Debris mound clinging to the east flank of the 1965 Hope slide scar at about the 4,700 ft. level. This material is a remnant of remaining debris from the earlier slide, as shown on pre-1965 aerial photographs of the area. The immaturity of vegetation on this mound and downslope from it suggest an age on the order of 100 years. It could be a relic of a small slide that might have resulted from the much larger earthquake that occurred Dec. 14, 1872 in this area (Mathews, 1969, pg. 70).

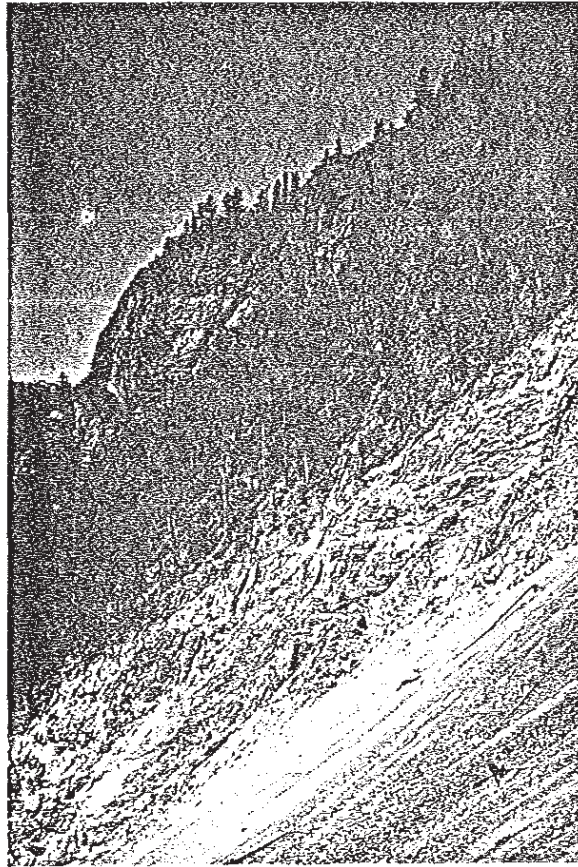


Figure 23

A photograph of the north flank of the Hope slide. The upper half is the exposed and weathered portion of the scarp of the ancient major slide, now overgrown. The lower half is fresh exposure due to the removal of slide debris by the 1965 event. As shown, the depth of this debris was on the order of 100 ft.



The Hope Slide  
Suggestions of Future Instability



Figure 24

This photo shows the existing headwall, or scarp of the Hope slide. Open cracks and loose rocks are plainly visible, indicating that weathering and erosion have not yet cleaned the failure-related, inadequately supported blocks from the scarp. Boulders and small slides still occur occasionally, sending material falling to the extensive debris slope below. Little, if any, of this rock falls the full 3,000 ft. to the valley bottom.



Figure 25

This photo contains almost all of the remaining important tension cracks behind the scarp of the 1965 slide. The parallelism and offsetting indicate that movement is occurring entirely along joints and not through intact rock.

The volume of material involved in this unstable area, if we assume a crack length of 100 m, depth of 40 m, and horizontal distance from the crest of 30 m, is  $120,000 \text{ m}^3$ , or about 250,000 metric tons.

The headwall block that fell causing the 1965 slide involved an estimated  $8,800,000 \text{ m}^3$  or about 18,000,000 metric tons. The volume of the zone shown above is less than about 1.5 per cent of the volume involved in this portion of the 1965 event.

It is Dr. Mathew's impression that the cracks shown above have widened significantly during the intervening years since his inspection following the 1965 slide.





Figure 26

The tension crack shown in this photo (angling sharply up to the left from the reddish brown coloration in the right center to the shadow three fourths of the way up the scarp and then exiting just to the left to the point on the skyline) is the only evidence of continuing major instability in the exposure of the 1965 failure. This crack is not related to prominent structure and thus probably portends a deep-seated, large scale earth movement. The direction of motion indicated is to the west and north west and thus, should the hill slide again, the debris would fall to the north of the existing debris pile in the valley below.

RELEASE OF CLAIMFor and in consideration of the Sum of Eleven thousand DollarsCents (\$11,000.00), ~~receipt of which is hereby~~~~acknowledged by me~~ Sunshine Valley Developments Ltdof 283 East 11th Avenueof Vancouver, B.C., V5T 2C4

being of lawful age, do hereby release, acquit and forever discharge, and by these presents do for ~~myself~~/ourselves, heirs, executors, administrators and assignees, release and forever discharge Her Majesty the Queen in right of the Province of British Columbia, her servants and agents, of and from any and all actions, causes of action, claims, demands, damages, costs, loss of services, expenses and compensation, which ~~we~~/WE ever had or now have, or which ~~we~~/WE or ~~my~~/our heirs, executors, administrators or assigns hereafter can, shall or may have by reason of any matter, cause or thing whatsoever existing up to the present time, and without restricting the generality of the foregoing from, or in any action, cause of action, claim or demand, in any way arising out of

Removal of debris which was created at the approx. time of theHope Slide throughout the owner's property in Sections 1,11,12,13, and 14,Township 4, Range 25, W6M and Sections 5,6 and 7, Twp.4,Rge.24,W6M and Sections31 and 32, Twp.3, Rge.24,W6M,along the Southern Trans Provincial Highway #3.which occurred on, or about, the 9<sup>th</sup> day of JAN. 1965at or near Hope, B.C.WITNESS ~~my~~/OUR HAND AND SEALTHIS 16<sup>th</sup> DAY OF MAY1981 at Sunshine ValleyFOR: SUNSHINE VALLEY DEV. LTD.

IN THE PRESENCE OF:

[Signature]

SIGNATURE:

[Signature]

(SEAL)

SIGNATURE:

(SEAL)

SIGNATURE:

(SEAL)

Mr. W. Solberg,

Subdivisions and Gazettes,  
Ministry of Highways and  
Public Works, BUILDINGS

Property Services

January 9th 78

P/N 22943

H.Q. 292

S.T.P. Highway Revised Area  
for Hope Slide Viewpoint

Pursuant to an agreement with Sunshine Valley Developments Ltd. as accepted by the Deputy Minister, it is requested that the Gazette Notice published July 5, 1973, establishing additional right-of-way for the Hope Slide Viewpoint (Plan R.S.3329-1) be amended to restrict the additional area to 1.40 acres as per a revised plan. The attached notice of amendment is submitted for your consideration and reference.

E. W. Pritchard,  
Property Negotiator

EWP/mmf  
Attach.

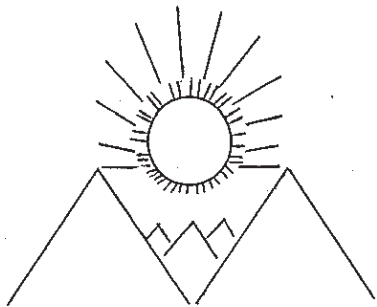


YALE LILLOOET ELECTORAL DISTRICT

Establishing additional Right-of-way for Southern Trans  
Provincial Highway Hope-Princeton Section

(Notice of Amendment)

NOTICE is given, pursuant to section 8 (2) of the Highway Act, and all other powers thereunto enabling, that notice dated June 28, 1973 and published in the British Columbia Gazette of July 5, 1973, and recorded in the Provincial Ministry of Highways and Public Works, Victoria, B. C. under Road Survey 3329-1 is hereby amended in accordance with a revised plan to cover an area of 1.40 acres within the South East  $\frac{1}{4}$  of Section 14, Township 4, Range 25, W6M.



# Sunshine Valley Developments Ltd.

January 17, 1974.

RECEIVED  
REGIONAL PROPERTY NEGOTIATION

JAN 21 1974

RECEIVED  
DEPT. OF HIGHWAYS  
BURNABY, B. C.

JAN 21 1974

Department of Highways,  
Regional Property  
Negotiations Branch,  
#302-4333 Ledger Avenue,  
Burnaby, British Columbia.  
V5G 3T3.

ANSWERED.....  
NOTED.....  
DATE.....

ANSWERED.....  
NOTED.....  
DATE.....

Attention: Mr. H.R. MacKenzie  
Regional Property Negotiator

Re: Your File No. P/N 22943-Yale-Lillooet  
Highway Re-location-

Dear Sirs:

This letter is in follow-up of yours of November 26, 1973, and of ours of November 2, 1973, wherein we have discussed having information concerning the re-routing of the #3 Highway, in the Hope Slide area.

It is important to us to know whether in fact this right-of-way is actually required by the Department of Highways, and if so, we should by now be entitled to information requested in our November 2, 1973, letter.

It is imperative that we have some answers now, so that we can proceed with our plans for work in this area for 1974.

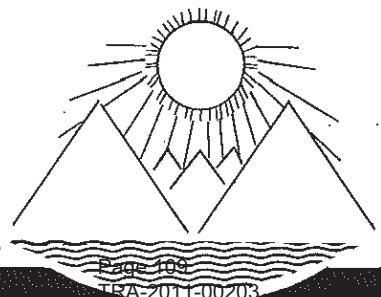
Your immediate attention to this matter would be appreciated, or if your considerations have become bottle-necked in Victoria, we would appreciate knowing who your correspondent is there, so that we may make a direct contact. Many thanks.

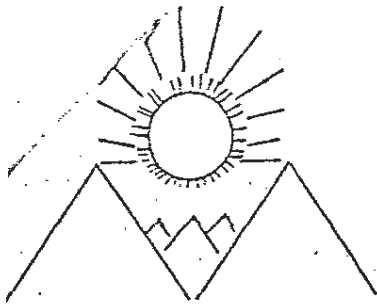
Yours very truly,

SUNSHINE VALLEY DEVELOPMENTS LTD.

  
Per: D.W. Low

DWL:aey





Sunshine Valley

Developments Ltd.

292

January 19, 1978

RECEIVED  
MINISTRY OF HIGHWAYS  
& PUBLIC WORKS

JAN 24 1978

Mr. R. G. Harvey ANSWERED *RGH*  
Deputy Minister NOTED  
Ministry of Highways *Feb 17/78*  
Parliament Bldgs.  
Victoria, B.C.  
V8V 2M3  
Ph: 387-3280

PROPERTY NEGOTIATIONS  
Department of Highways

JAN 25 1978

Ans'd \_\_\_\_\_  
Victoria, B.C. File \_\_\_\_\_

Dear Mr. Harvey:

Re: Number Three Highway Right-of-way through Sunshine Valley

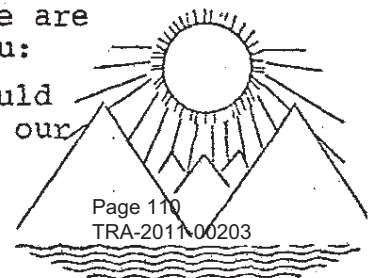
Thanks for the set of Highway right-of-way plans which arrived recently.

There are two items about those plans which we want to draw to your attention:

1. Re: Fourteen-Mile Creek right-of-way - we don't understand why you have to have all of the right-of-way ranging from 50 to 75 feet wide from the culverts passing under our private roadway just to the north of the highway, up to the point of diversion of our waterworks. There is now a wooden tank at the creek, at that point, which we may or may not use, but during 1978 we will probably have to connect to this creek in order to guarantee our domestic water supply. We would have no objection to your Department changing the course of the creek at the point of diversion down to the highway, as noted in the plans, but we would prefer that the land involved remain our own.

If it is absolutely necessary for your Department to have this creek right-of-way then there are some items that we should clarify with you:

- a) we would want a guarantee that we would be able to use this right-of-way for our



Page 110  
TRA-2011-00203

342778

V.A. Drew  
1/25



own waterworks line, AND WORKS.

- b) we would want to retain the right to fence the intake area near the point of the existing wooden water tank, in order to protect the water shed.
  - c) we would expect your Department to declare that the area above the highway would be out of bounds to the public.
  - d) we would want a priority arrangement to connect to and take water from this Fourteen-mile creek in 1978, if required.
2. Your right-of-way plans show fencing for only part way through our valley. We think that fencing is required for the whole length of our valley, for safety purposes. A fence will no doubt deter pedestrians, bike-riders, motorcycle trail riders, snowmobilers, and horse riders from crossing the highway at any number of points along it. We would say that there is urgency about keeping snowmobiles and horses from crossing anywhere but at designated crossings or underpasses. We would have great fears for the safety of children in particular, who decide to cross the highway at almost any point.

No doubt complete fencing could be worked into the plan.

Many thanks.

Yours very truly,

SUNSHINE VALLEY DEVELOPMENTS LTD.

  
Per: D. W. Low  
President

sh

# MINISTRY OF HIGHWAYS AND PUBLIC WORKS (HIGHWAYS)

<b>TO:</b> Regional Highway Engineer Ministry of Highways and Public Works, KAMLOOPS	<b>SENDER'S ADDRESS:</b> Victoria, B. C.  <b>DATE:</b> January 23, 1978  <b>DISTRICT:</b>  <b>HEADQUARTERS FILE:</b> P/N 22943  <b>REGIONAL FILE:</b>
<b>ATTENTION:</b> R. E. Burnett, <u>Regional Property Negotiator</u>	<div style="border: 1px solid black; padding: 2px; text-align: center;">             Regional Engineer's Office              DEPARTMENT OF HIGHWAYS           </div>
<b>SUBJECT:</b> Hope - Princeton Highway, and Sunshine Valley <u>Developments Ltd.</u>	<b>REFERENCE:</b>  <div style="border: 1px solid black; padding: 5px; text-align: center;">             JAN 25 1978           </div>
<div style="border: 1px solid black; padding: 5px; display: inline-block;">             KAMLOOPS, B. C.           </div>	

The enclosed copy of H - 120 and amending Gazette  
 Notice establishing viewpoint is for your file.

A. C. Brown,  
 Assistant Director of  
 Property Services

ACB/rmf  
 Attach.

c.c. L. H. Mercier, Regional Property Negotiator,  
 Ministry of Highways and Public Works, BURNABY

- attached is for your records.

A. C. B.

FOR DEPARTMENTAL CORRESPONDENCE ONLY.

## MEMORANDUM

Mr. R.G. Harvey,

Deputy Minister,

Ministry of Highways &amp; Public Works.

FROM

Director of Highway Design &amp; Surveys.

February 1, 1978

SUBJECT Highway 3, Hope - Princeton Slide Section.

OUR FILE L2-03-378

YOUR FILE P/N 22943  
H.Q. 292/342778

Comments on Mr. Low's letter of January 19, 1978 are as follows:

1. Fourteen Mile Creek

\* The boundaries shown for Fourteen Mile Creek define the Clearing and Grubbing area necessary to re-establish the old channel. It is not the intent the area be purchased as R/W, but we will require permission to work within the area. Portion of Drawing 11 is attached - note the different line symbol used for the Clearing and Grubbing boundary.

2. Fencing

\* We have basically shown fencing replaced in the areas of existing fencing. Drawing 9 has now been amended to also include fencing on the south side from sta. 225 to 247. (See print attached)

To fence the complete R/W (say from Viewpoint to sta. 316) as Mr. Low suggests would require an additional 1560 rods at an estimated cost of \$55,000. Through most of this remaining area, either the topography or the proximity of creeks make crossing of the R/W difficult.

*E.E. Readshaw*  
for E.E. Readshaw,  
Director of Highway  
Design and Surveys.

MSI:nlp  
Encl.



**YALE-LILLOOET ELECTORAL DISTRICT**

Establishing Additional Right-of-way for  
Southern Trans-Provincial Highway  
Hope to Princeton Section

(Notice of Amendment)

NOTICE is given, pursuant to section 8 (2) of the *Highway Act*, and all other powers thereunto enabling, that notice dated June 28, 1973, and published in *The British Columbia Gazette* of July 5, 1973, and recorded in the Provincial Ministry of Highways and Public Works, Victoria, B.C., under "Road Surveys 3329-1," is hereby amended in accordance with a revised plan to cover an area of 1.4 acres within the southeast quarter of Section 14, Township 4, Range 25, W6M.

ALEX. V. FRASER  
Minister of Highways and  
Public Works

Ministry of Highways and  
Public Works,  
Parliament Buildings,  
Victoria, B.C., January 19, 1978.  
File 292/342460 fe2—1098

**KOOTENAY ELECTORAL DISTRICT**

Establishing Right-of-way for Wardner-  
Fort Steele Highway, Bull River  
North Section

NOTICE is given, pursuant to section 8 of the *Highway Act*, that all those portions of the south half and Lot A, Plan 5085, both in District Lot 676; Sublot 1, Plan X40, District Lot 312; District Lot 106, Sublots 3 and 6, Plan X40, District Lot 313; District Lots 2959, 4590, and 2960; Lot 1, Plan 4226, District Lot 2960; and District Lot 3705, all in Kootenay District, as shown outlined in red on a plan of survey recorded in the Provincial Ministry of Highways and Public Works, Victoria, B.C., under "Road Surveys 5357" (being a copy of a plan deposited in the Nelson Land Registry Office as Plan 7690), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Highways and  
Public Works

Ministry of Highways and  
Public Works,  
Parliament Buildings,  
Victoria, B.C., January 19, 1978.  
File 487/342147 fe2—1098

**YALE-LILLOOET ELECTORAL DISTRICT**

Establishing Right-of-way for  
Trans-Canada Highway

NOTICE is given, pursuant to section 8 of the *Highway Act*, that all those portions of District Lot 28, Yale-Lytton District (except plan attached to DD 2914b); District Lot 1260, KDYD; District Lot 379, KDYD (except part shown on Plan B540); Legal Subdivision 12, Section 31, Township 17, Range 24, W6M, which lies to the west of the right bank of the Thompson River (except plan attached to DD 3995r); the northwest quarter of Section 31 and the fractional east half of Section 7, both Township 18, Range 24; District Lot 500, KDYD; fractional Legal Subdivision 7 of the fractional southeast quarter of Section 18 and the northeast quarter of Section 18; the fractional southeast quarter, the fractional northeast quarter,

the fractional northwest quarter of Section 19; the fractional northwest quarter of Section 30; and the fractional southwest quarter of Section 31, all in Township 18, Range 24; the southeast quarter and the northwest quarter of Section 36, both in Township 18, Range 25; the fractional southeast quarter, the southwest quarter, and the northwest quarter of Section 6, all in Township 19, Range 24, as shown outlined in red on a plan recorded in the Provincial Ministry of Highways and Public Works, Victoria, B.C., under "Road Surveys 5375" (being a copy of Highway Design and Surveys Drawings R2-20-02 to R2-20-27), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Highways and  
Public Works

Ministry of Highways and  
Public Works,  
Parliament Buildings,  
Victoria, B.C., January 26, 1978.  
File 533/342351 fe2—1098

**NELSON-CRESTON ELECTORAL DISTRICT**

Establishing Additional Right-of-way for  
Kenville Road 181

NOTICE is given, pursuant to section 8 of the *Highway Act*, that all that part of Parcel 3 (see 27768r), District Lot 6890, Kootenay District, as shown outlined in red on a plan recorded in the Provincial Ministry of Highways and Public Works, Victoria, B.C., under "Road Surveys 5374" (being a copy of Highway District Plan 1-374), is hereby established as public highway.

ALEX. V. FRASER  
Minister of Highways and  
Public Works

Ministry of Highways and  
Public Works,  
Parliament Buildings,  
Victoria, B.C., January 26, 1978.  
File 1667/342546 fe2—1098

**MOTOR-VEHICLE ACT**

PURSUANT to section 140 (3a) of the *Motor-vehicle Act* the following area is defined as a speed zone area:

1862 Lower Summer Subdivision Kimberley—the lands lying within District Lots 11598 and 15301 (excluding that part of District Lot 11598 lying within the City of Kimberley).

ALEX. V. FRASER  
Minister of Highways and  
Public Works

Ministry of Highways and  
Public Works,  
Parliament Buildings,  
Victoria, B.C., January 19, 1978.  
File 5394-13 fe2—1098

**BOUNDARY-SIMLKAMEEN ELECTORAL DISTRICT**

Classification of Highways, City of  
Greenwood

NOTICE is given, pursuant to sections 31 and 32 of the *Highway Act*, that the undernoted change has been made with respect to the classification of highways within the municipal area of the City of Greenwood, namely:

1. That, by Order in Council 42, approved and ordered January 12, 1978, the arterial classification of Trans-Provincial

Highway, Reference No. 43R, is rescinded as follows:

Reference No. 43R, *Trans-Provincial Highway*—Trans-Provincial Highway, from the south boundary of City of Greenwood to the north boundary of the City of Greenwood, a total distance of 1.75 miles, more or less.

2. That, by Order in Council 3651, approved and ordered December 8, 1977, the following described highway is classified as an arterial highway, namely:

Reference No. 619R, *Southern Trans-Provincial Highway*—Southern Trans-Provincial Highway, commencing at the westerly boundary of the City of Greenwood, thence in a generally northerly direction to the northerly boundary of the City of Greenwood, a total distance of 2.1 miles (3.38 km), more or less.

ALEX. V. FRASER  
Minister of Highways and  
Public Works

Ministry of Highways and  
Public Works,  
Parliament Buildings,  
Victoria, B.C., January 26, 1978.  
File 5450-2/328779 fe2—1099

**NORTH VANCOUVER-SEYMOUR AND NORTH VANCOUVER-CAPILANO ELECTORAL DISTRICTS**

Classification of Highways, District of  
North Vancouver

NOTICE is given, pursuant to sections 31 and 32 of the *Highway Act*, that the undernoted change has been made with respect to the classification of highways within the municipal area of The Corporation of the District of North Vancouver, namely:

1. That, by Order in Council 3795, approved and ordered December 15, 1977, the arterial classification of Trans-Canada Highway, Reference No. 328R, is rescinded as follows:

Reference No. 328R, *Trans-Canada Highway*—Trans-Canada Highway, from the west boundary of the District of North Vancouver in Lot A, District Lot 607, Group 1, NWD; thence in a general easterly direction to the east boundary of Lot 72, District Lot 598, Group 1, NWD; thence from the west boundary of Blocks 7 and 12, District Lot 2025, Group 1, NWD, in a general southeasterly direction to the south boundary of Block 12, District Lot 2025, Group 1, NWD; thence from the north boundary of Block 111, District Lot 553, Group 1, NWD, in a general easterly and southerly direction to the south boundary of the District of North Vancouver, in District Lot 193, Group 1, NWD, including various cloverleafs, and having a total length of 2.92 miles, more or less; the Keith Road Interchange, a distance of 0.48 mile, more or less; the Capilano Interchange, a distance of 0.37 mile, more or less; and a portion of the Lynn Valley Interchange, a distance of 0.04 mile, more or less.

2. That, by Order in Council 3796, approved and ordered December 15, 1977, the following described highway is classified as an arterial highway, namely:

Reference No. 611R, *Trans-Canada Highway*—Trans-Canada Highway, from the west boundary of the District of North Vancouver in Lot A, District Lot 607, Group 1, NWD; thence in a generally easterly direction to the east bound-

xxxxxxxxxxxx

7818 - 6th Street

Burnaby, B.C.

V3N 4N8

Telephone : 525-0961

P/N 22943

Yale-Lillooet

February 8th 1980

Sunshine Valley Developments Ltd  
283 East 11th Avenue  
Vancouver, B.C.  
V6T 2C4

Attention : Don W. Low  
President

Dear Sir,

Re: Southern Trans Provincial Highway  
No. 3 - Project #4477  
Equestrian and Pedestrian Underpass  
Ministry Location Drawing  
R2-22-10 Station 256

Confirming our telephone conversation February 8th 1980,  
and your approval that the Ministry will be ditching your  
private lands across L.S. 12 of the NW $\frac{1}{4}$ , Section 6, Township 4,  
Range 24 to facilitate construction of the aforesaid underpass,  
which is also covered by agreement dated June 26th 1978 but  
nevertheless assuring that temporary fencing or gates will be  
provided for stock control if and where required. Thank you.

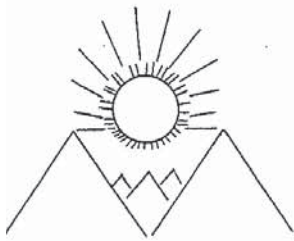
Yours truly  
Lionel H. Mercier  
Regional Property Negotiator

By:

J.D. Killer  
Property Negotiator

JDK/kbv

c.c. Regional Highway Engineer, Burnaby, B.C.  
c.c. Regional Construction Engineer, Burnaby, B.C.  
c.c. District Highways Manager, Chilliwack, B.C.  
Attention : D. Kirkland



# Sunshine Valley Developments Ltd.

July 18, 1980.

RECEIVED  
REGIONAL PROPERTY NEGOTIATOR

(JUL 17 1980)

ANSWERED.....  
NOTED.....  
DATE.....

Ministry of Highways,  
7818 Sixth Street,  
Burnaby, B.C.  
V3N 4N8, Phone: 525-0961

Attention: Mr. Jim Killer - Highway Negotiator

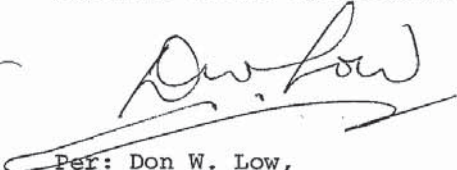
Dear Sirs:

Enclosed herewith is a copy of Gazette notice on page 312 of the British Columbia Gazette, dated February 22, 1979, showing the "Establishing Right-of-way for Sunshine Valley Road".

We would appreciate having full particulars of what this Gazette note covers. Many thanks.

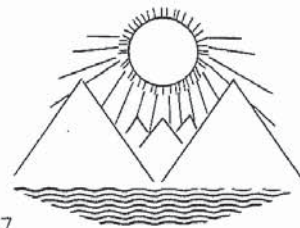
Sincerely,

SUNSHINE VALLEY DEVELOPMENTS LTD.,

  
Per: Don W. Low,  
President

DWL/b

Encl.



283 East 11th Avenue, Vancouver, Canada V5T 2C4 (604) 873-3377



W6M, KDYD, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation, Communications and Highways, Victoria, B.C., under "Road Surveys 5920" (being a copy of Highway District Plan D-146-2), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation,  
Communications and  
Highways  
Ministry of Transportation,  
Communications and Highways,  
Parliament Buildings,  
Victoria, B.C., February 15, 1979.  
File 21-20-02/337019 fe22-2281

#### SURREY ELECTORAL DISTRICT

Project 3983—Reconstruction: Fraser Highway 1A to 62nd Avenue on King George V Highway 99A

CLOSING DATE for receipt of tenders is extended to Tuesday, February 20, 1979, at 2 p.m.

R. G. HARVEY  
Deputy Minister  
February 12, 1979.  
File 14-01-54 fe22-2280

#### SURREY ELECTORAL DISTRICT

Establishing Right-of-way for Fraser Highway Vicinity of 160th Street (Pike Road)

NOTICE is given, pursuant to section 8 of the Highway Act, that all those portions of Parcel A (Exp. Plan 10962) of Lots 39 and 41 of Blocks 2 and 7 of the northeast quarter of Section 26, Plan 2824; Parcel A (Exp. Plan 10841) of Lot 40 of the northeast quarter of Section 26, Plan 2824; Lot 40 of the northeast quarter of Section 26, Plan 2824 (except Parcel A (Exp. Plan 10841)); Lot 24 of the northwest quarter of Section 25, Plan 26463; Lot J of Parcel B of the northwest quarter of Section 25, Plan 19257; Lot 5 of the northwest quarter of Section 25, Plan 9175, all Township 2, N.W.D., as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation, Communications and Highways, Victoria, B.C., under "Road Surveys 5921" (being a copy of Highway District Plan 30-14 R/W), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation,  
Communications and  
Highways  
Ministry of Transportation,  
Communications and Highways,  
Parliament Buildings,  
Victoria, B.C., February 15, 1979.  
File 14-20-01/337268 fe22-2282

#### MOTOR-VEHICLE ACT

PURSUANT to section 140 (3a) of the Motor-vehicle Act, the following area is defined as a speed zone area:  
1923 Hendrix Lake Townsite—The land lying within Plan 14605, Lot 12046, Cariboo District.

ALEX. V. FRASER  
Minister of Transportation,  
Communications and  
Highways  
Ministry of Transportation,  
Communications and Highways,  
Parliament Buildings,  
Victoria, B.C., February 15, 1979.  
File 70-00-24 fe22-2282

#### YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right-of-way for Trans-Canada Highway, Vicinity of Dogwood Valley

NOTICE is given, pursuant to section 8 of the Highway Act, that all that part of Lot 1, Section 22, Township 6, Range 26, W6M, Plan 3137, YDYD (except Plan 13685), as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation, Communications and Highways, Victoria, B.C., under "Road Surveys 5917" (being a copy of Highway District Plan 174-2), is hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation,  
Communications and  
Highways  
Ministry of Transportation,  
Communications and Highways,  
Parliament Buildings,  
Victoria, B.C., February 15, 1979.  
File 15-20-03/336735 fe22-2282

#### YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right-of-way for Kamuse Road 241

NOTICE is given, pursuant to section 8 of the Highway Act, that all that part of District Lot 933, YDYD (except Plan 10227), as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation, Communications and Highways, Victoria, B.C., under "Road Surveys 5916" (being a copy of Highway District Plan 54-12), is hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation,  
Communications and  
Highways  
Ministry of Transportation,  
Communications and Highways,  
Parliament Buildings,  
Victoria, B.C., February 15, 1979.  
File 25-20-02/336736 fe22-2282

#### YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right-of-way for Sunshine Valley Road

NOTICE is given, pursuant to section 8 of the Highway Act, that all those portions of District Lot 1022, Plan B1111; District Lot 1022 (except Parcel B, Plan B1111 and Plan 23138); and the southeast quarter of Section 1, Township 14, Range 23, W6M, lying north of the right bank of the Nicola River (except plan attached to DD 10725 and Plan 11246), all Kamloops Division of Yale District, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation, Communications and Highways, Victoria, B.C., under "Road Surveys 5924" (being a copy of Highway District Plan 39-12), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation,  
Communications and  
Highways  
Ministry of Transportation,  
Communications and Highways,  
Parliament Buildings,  
Victoria, B.C., February 15, 1979.  
File 25-01-14/337265 fe22-2282

### LEGISLATIVE ASSEMBLY

#### PRIVATE BILLS

Excerpts From Standing Orders Relating to Private Bills

97. No petition for any Private Bill shall be received by the House after the first 10 days of each Session, nor may any Private Bill be presented to the House after the first three weeks of each Session, nor may any report of any Standing or Select Committee upon a Private Bill be received after the first four weeks of each Session, and no motion for the suspension or modification of this Standing Order shall be entertained by the House until the same has been reported on by the Committee on Standing Orders, or after reference made thereof at a previous sitting of the House to the Standing Committees charged with the consideration of Private Bills, who shall report thereon to the House. And if this rule shall be suspended or modified as aforesaid, the promoters of any Private Bill which is presented after the time hereinbefore limited, or for which the petition has been received after the time hereinbefore limited, shall in either case pay double the fees required in respect of such Bill by this rule, unless the House shall order to the contrary.

98. (1) Any person seeking to obtain any Private Bill shall deposit with the Clerk of the House, eight clear days before the opening of the Session,

- (a) a printed copy of such Bill;
- (b) a copy of the petition to be presented to the House;
- (c) copies of the notices published, with publication proved by a statutory declaration or affidavit to the satisfaction of the Clerk of the House.

(2) At the time of depositing the Bill and petition the applicant shall also pay to the Clerk of the House the sum of \$300.

(3) If a copy of the Bill, petition, and notices proved as aforesaid shall not have been so deposited in the hands of the Clerk of the House at least eight clear days before the opening of the Session or if the petition has not been presented to the House within the first 10 days of the Session, the amount to be paid to the Clerk shall be \$600. If the Bill shall not pass second reading, one-half of the fees shall be returned.

100. (1) All applications to the Legislative Assembly for Private Bills of any nature whatsoever shall require the publication of a notice clearly and distinctly specifying the nature and object of the application, and when the application refers to any proposed work, indicating sufficiently the location of the work, to be signed by or on behalf of the applicant.

(2) Such notice shall be published in *The British Columbia Gazette* and once a week in one leading daily newspaper freely circulating in all parts of the Province. The notice in such newspaper to appear on Wednesday of each week.

(3) When the proposed Bill is of such a nature that it may particularly affect one or more electoral districts, the notice shall furthermore be published once a week if possible in some newspaper in every electoral district affected, or if there be no newspaper published therein, then in a newspaper published in the electoral district nearest thereto.

COPY



DEPARTMENT OF HIGHWAYS

V8V 2M3

February 25, 1975.

TO ENSURE IMMEDIATE ATTENTION  
PLEASE QUOTE FILE NO.....

Yale Lillooet

11m 22943

Russell & DuMoulin,  
Barristers & Solicitors,  
17th Floor, MacMillan Bloedel Building,  
1075 West Georgia Street,  
Vancouver, British Columbia.  
V6E 3G2.

Attention: Mr. Allan McEachern

Dear Sirs:

Re: Sunshine Valley Development  
Hope Slide Viewpoint

In response to your letter of February 19, 1975, I can advise that the matter has been delayed pending receipt of a final plan from our Design and Surveys Branch covering the relocation of the highway through the above owners' lands. The plans have been delayed somewhat because our design people have been endeavouring to solve environmental problems such as stream interference and yet comply as much as possible with the wishes of the owners of Sunshine Valley Development. I expect that we could be in a position to deal with the whole matter shortly after the snow goes this Spring. I believe that there are some advantages to Sunshine Development with the upgrading of the highway and these should be given account in any settlement made with the owners for the viewpoint.

Yours very truly,

RECEIVED  
REGIONAL PROPERTY NEGOTIATOR

N. C. Tattrie,  
Chief Property Negotiator.

FEB 26 1975

NCT:km

c.c. Regional Property Negotiator,  
BURNABY.

ANSWERED.....  
NOTED.....  
DATE.....



March 21/79

# Floods-Hope Highway to Be Ready When Coquihalla Done

(Continued from Page 1)

Hon. Tom Waterland, Minister of Forests and M.L.A. for Yale-Lillooet, and Hon. Alex Fraser, Minister of Transportation, Communication and Highways fielded a variety of questions when they met with about 40 Hope and District residents in Hope United Church hall on Saturday, March 17, at 7:30 p.m.

Mr. Waterland and Mr. Fraser were introduced by Inez Eaton, president of the Hope Social Credit group.

In his opening remarks, Mr. Waterland talked about many areas of government.

In discussing Premier Bill Bennett's decision to give every qualifying B.C. resident five free shares of the B.C. Resources Investment Corporation, Mr. Waterland pointed out that only 60,000 of nearly 2.4 million B.C. residents have owned any shares or dealt in corporation shares.

He cited the share offer as a good investment and said that "I will buy every share that I can possibly afford".

He said that his government's decision to return B.C.R.I.C. to public ownership was based on the idea that the public would benefit directly from the investment.

He criticized the previous N.D.P. government saying that they acquired the property that makes up B.C.R.I.C. holdings because they believe "that really, you're not bright enough to manage your own affairs."

Mr. Waterland also touched on B.C. Federation of Labor's accusations that the government is going to institute right-to-work legislation.

"Our government has no intention and is not considering instituting right-to-work legislation in the near future."

He said that although there is an election in the offing, right-to-work legislation will not be an election issue.

After being introduced by Mr. Waterland, Mr. Fraser outlined his responsibilities as Minister of Transportation, Communications and Highways.

The minister said that the main highway system in the province is in "deplorable condition" but added that there is an ongoing program designed to upgrade the roads.

Mr. Fraser also said that the transportation system on the Coast needs up-grading and pointed out that "coastal communities like Bella Bella have fifth class transportation systems, believe me."

He said that the problem of insufficient transportation on the coast became more obvious when Northland Navigation discontinued regular ferry services to many outlying communities.

Mr. Fraser told the meeting that he was present at the opening of the first tender for construction of the Coquihalla Highway early in December, and was pleased to see that Emil Anderson Construction Co. Ltd. of Hope submitted the lowest bid. He said that the bid was for \$4.2 million for the first 3.0 miles. He indicated that inflation was one reason why construction costs were so high, but pointed out that when the Fraser Canyon highway was being built, in the 1950's construction costs were estimated

at a fantastic \$1,000,000 per mile.

Although he said that he was "not making any promises as to when the next contract will be let", Mr. Fraser re-assured his audience that he expected the highway to be completed within eight years.

The Highways minister told the meeting that contracts for paving five miles of road near Jones Creek bridge and passing lanes from American Creek to Spuzzum had been awarded. He said that his ministry is trying to encourage the contractor to start as soon as possible now that spring has arrived.

In an interview later, Mr. Fraser was asked when construction of the four-lane highway between Flood and Hope

(Continued on Page 8)

During a question period Robert Swinhoe of the Hope and District Chamber of Commerce asked Mr. Fraser if anything could be done to clean up unsightly properties around Hope.

"Two of them belong to you," he said, referring to the former Pacific 66 and Ken's Other Esso service stations situated at the west entrance to Hope.

Mr. Swinhoe said that the two lots were littered with old cars, broken glass and junk; and would be an influence to vandalism. He said that the sites were not good advertising for the town and asked if something could be done about it.

"It will be cleaned up, sir, forthwith," Mr. Fraser said.

Alderwoman Mildred Ehlers directed a question to Mr. Waterland. She asked him if the provincial government would auction off Coquihalla Park if the Town of Hope would not renew the lease. She said that the town had been leasing the park for over 20 years.

Mr. Waterland was adamant when he said that he would not get involved in the situation as it would be solved by the local council, and reminded Mrs. Ehlers that the land had been leased as a park, but had not been designated as a park. He

would begin. He said that the Coquihalla highway was first on the list of local jobs, but added that by the time the new highway is built, the sections from Hope to Flood, and Laidlaw to Jones Creek hill would have to be completed. He also said that 12 to 75 parcels of land affected by the highway between Flood and Hope still have to be negotiated for.

said that the government would auction off the land if the town would not agree to lease it again, but again said that he would not get involved in the dispute.

Mayor Keith Gardner, who was present at the meeting, agreed with Mr. Waterland and said that the problem was local and that the minister should not become involved.

Mr. Gardner said that at a special council meeting on Wednesday, March 14, town council had agreed to apply for a continued lease although the land is currently zoned for industrial use. He said that the future of the park area remains to be foreseen.

Mr. Waterland also fielded other questions. Alfred Schaer of North Bend asked when and if a bridge would be built across the Fraser River to North Bend.

Mr. Waterland said that studies are continuing and that an outside consultant is currently working on the project. He said that the possibility of a bridge being constructed hinges on the local forest economy. He also said that Mr. Fraser had indicated that the cost of running the North Bend Ferry was around \$110,000. Mr. Waterland suggested that the cost of transporting logs across the river to the local mill was just as much.

B.C. Forest Products, which owns the mill, has agreed to pay for part of the cost of constructing a single-lane bridge, Mr. Waterland said. Cost of the bridge has been estimated at \$1.5 million.



ASSIGNMENT  
MINISTRY OF HIGHWAYS AND PUBLIC WORKS

To LIONEL MERLIERDate APRIL 7/81File R1-0-21 SP. 1090

Letter No. \_\_\_\_\_

SUBJECT CLAIM - DON LOW  
DEBRIS CLEAN-UP

**RECEIVED**  
REGIONAL PROPERTY NEGOTIATOR

- |   |   |
|---|---|
| 1. Please investigate and report to this office.    | 9. Please reply direct.                       |
| 2. Please investigate and take action as necessary. | 10. cc to this office.                        |
| 3. Comments, please.                                | 11. Please prepare reply for _____ signature. |
| 4. Recommendations, please.                         | 12. For your attention, please.               |
| 5. Discuss, please.                                 | 13. For your signature, please.               |
| 6. Note and return, please.                         | 14. For your information, please.             |
| 7. Note and forward, please.                        | 15. For your file, please.                    |
| 8. Note and forward to file, please.                |   |

REMARKS SUGGEST SIM KILLER CONTACT  
LOW TO OFFER CASP SETTLEMENT.  
I WOULD BE PREPARED TO ATTEND  
FIRST MEETING.

FIRST OFFER SHOULD BE \$10,000.  
8/1/4/28 - Spoke to Mike who advised he had  
offered how \$10,000 & mentioned possible  
trade-off awaiting how response. Hold  
till advised.

From 



H118

To: Regional Highway Engineer  
Ministry of Transportation and Highways  
Burnaby

Date: March 26, 1981

RECEIVED  
DEPT. OF HIGHWAYS  
VICTORIA, B.C.

Headquarters File: 16-00-01

Regional File: P/N22943

APR 1 1981

District File:

Re: Sunshine Valley Developments Ltd.

Referring to your memorandum of November 3, 1980, the instructions from Mr. A. E. Rhodes, Assistant Deputy Minister, are that it is in order to negotiate a cash settlement with Mr. Low for a release at some figure under \$12,000.00.

G. A. Cavin  
Insurance and Claims Officer

GAC:MK

8/14/16 - DUE TO YOUR ABSENCE I WAS UNABLE TO SUGGEST AN EXCHANGE FOR THE VIEWPOINT LANDS INSTEAD OF MONETARY COMPENSATION FROM EITHER PARTY & WOULD APPRECIATE YOUR THOUGHTS ON THIS. I WILL BE AWAY UNTIL APR. 27/81 & WILL DEAL WITH THE MATTER THEN AFTER I HAVE CONSULTED WITH YOU.

XXXXXXXXXXXX

G.A. Cavin,  
Insurance & Claims Officer,  
VICTORIA, B.C.

November 3, 1980

R1-0-21 SP. 1090

RE: CLAIM - DON LOW  
DEBRIS CLEAN-UP

Attached please find relevant correspondence on this matter.

When the Hope Slide occurred in 1965 we built the present detour in something like 12 days. Some slash and debris was piled along the road and was not disposed of. When a right-of-way was set some years later, some of these piles ended up outside the right-of-way.

As I see it we have two problems here. Firstly, what responsibility did we have to the original owner of this property to clean up the slash and debris. If our responsibility was limited then we have no responsibility to Low's. If our responsibilities were not fulfilled to original owner, what responsibility do we have to Low's?

Secondly, Low purchased the property many years ago. Do we have any responsibility to a new owner for this kind of thing?

If we have to clean the piles up, we would be looking at in excess of \$20,000. Low will consider \$12,000., possibly less, as a settlement.

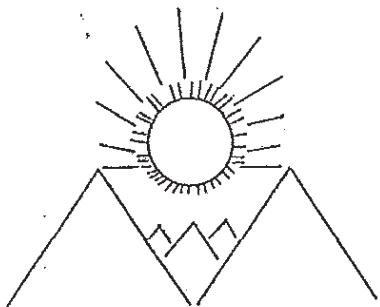
As you will note I wrote to Low in August 1980, and advised that we would contact him after you and possibly our solicitor had a look at the matter. Since August we have researched all our old files and obtained a better idea of where the debris is.

If you have any questions please contact me, and I would suggest you contact me before contacting Low's,

M.J. O'Connor,  
Regional Highway Engineer

MJO'C:jb  
Attach.





# Sunshine Valley

## Developments Ltd.

RECEIVED  
DEPT. OF HIGHWAYS  
BURNABY, B.C.

JUN 25 1979

ANSWERED \_\_\_\_\_  
NOTED \_\_\_\_\_  
DATE \_\_\_\_\_

June 22, 1979.

Department of Highways and Public Works,  
7818 - 6th Street,  
Burnaby, B.C.  
V3N 4N8 PHONE: 525-0961

re: Your File 16-21-78-34 and 16-21-78-35 and Regional District  
of Fraser Cheam letter of June 6, 1979.

Attn: Mr. M. O'Connor, P.Eng.  
Regional Highways Engineer

Dear Sirs:

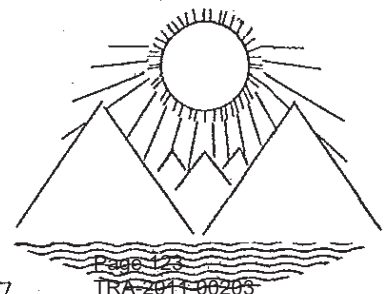
1. 123 LOT BARE LAND STRATA PLAN IN L.S. 7, 8, 9, 10 - Your  
Reference 16-21-78-34

We want to emphasize that our application is not a deeded subdivision, but rather a Bare Land Strata Plan, within which all of the services including roads are maintained by the Strata Company. We refer to Mr. Norman Wells' letter dated 30 May 1979 regarding 50' road widths. Under the Strata Plan we are still prepared to develop roads with 50' right-of-ways. We notice that two Bare Land Strata Plans which were filed in Kamloops and signed by your Department have roads of 50' and 3 metres wide. Mr. Hunter or Mr. Paulson of your office have copies of these plans, and we feel that our 50' road widths should be acceptable under the Bare Land Strata Plan.

Paragraph 1 of Mr. Wells' letter refers to the existing entrance road as having a 17% grade, and we want to resolve something concerning this problem. When this roadway was first built it was approved by your Department, and if necessary, we will take the maintenance of the road in order to relieve Highways of this problem.

In paragraph 2 of Mr. Wells' letter he refers to a maximum of 8% grade, but we were advised by Highways personnel that

.....



we could probably go to 10% grade, and 10% is presently the maximum planned grade within the Bare Land Strata Plan. We think Highways Department should be able to live with this as a Bare Land Strata.

With respect to off-street parking, we would like to show you upon the ground how we would intend to prepare for that parking within the Strata Plan.

We would like to discuss on the grounds the item contained within paragraph 6 of Mr. Wells' letter regarding roadside slope gradiance.

We would also like to discuss with you paragraph 7 of Mr. Wells' letter regarding the possibility of flooding in the Trites Creek gulley.

2. STRATA PLAN - L.S. 1 - Your Reference 16 - 21 - 78 - 35

Regarding paragraph 1 of Mr. Norman Wells' letter of May 30th, 1979 we would like to discuss the private access from Forestry access road to the bridge across Sumallo River.

We would like to discuss on site, paragraph 6 of Mr. Wells' letter regarding the Strata Plan and not a Bare Land Strata Plan. Under our Strata Plan application we show public parking, no road frontages to each building, and no sub-division within the Strata Plan. It is our intention to construct support buildings as a normal Strata Plan.

We would like to discuss the geotechnical problem on site to show how that our Strata Plan is designed to avoid any problem areas.

3. OUR APPLICATION TO ACQUIRE FROM THE CROWN 2.7 ACRES ADJACENT TO THE HOPE SLIDE VIEWPOINT (Refer to Fraser-Cheam letter Attached)

We applied in November 1978 to the Lands Branch to acquire the 2.7 acres, and we have also applied to the Regional District to have our land and this portion zoned C-3 (commercial). The Lands Branch advise us that they have not heard from you, and the Regional District advise us that your representative did not appear at the June 11th meeting of the Technical Planning Committee meeting to discuss this item.

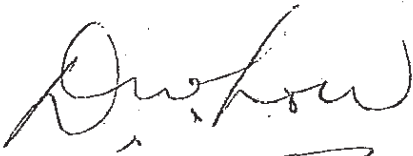
.....

We have also applied to have 9 acres zoned commercial (C2) alongside of the existing Highway 3 where it crosses from the south to the north side of our Valley directly opposite our entrance-way. The Regional District Board have passed this application in the first reading and requested a clearance from Highways Department regarding ingress and egress to the west side building site, from Highways Department, but apparently no one was in the meeting of June 11 to discuss the matter. Mr. Orr suggested that the next meeting would be July 9, 1979, but we think it would be helpful for you to see this situation on the site, prior to that meeting.

4. Mr. Wells suggested that we contact you and meet with you and him on the site to go over these problems, in order to resolve them. We would appreciate you giving us a date as soon as possible when you could be at Sunshine Valley with Mr. Wells, and/or Mr. Lloyd Paulson, or whoever, during the first week of July, 1979, as you indicated to me on the phone on Friday, 22 June 1979. You mentioned to me on the phone on Friday, 22 June 1979, that Highways Department wanted to reserve the 2.7 acres in our Viewpoint application, for expansion of the Viewpoint, for Highways purposes, and since this does not comply with the spirit of our negotiations with your Department and in particular with Mr. Bob Harvey, Deputy Minister, in Victoria, we will probably be in touch with him about this matter prior to our meeting at Sunshine Valley.

Yours truly,

SUNSHINE VALLEY DEVELOPMENTS LTD.,



Per: Don W. Low  
President

DWL/b

Encl.





To: Administration Division  
Ministry of Transportation,  
Communications and Highways  
940 Blanshard Street  
Victoria, B.C.

Date: 79/6/22

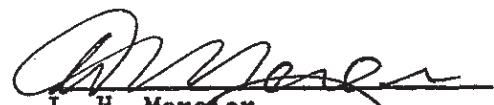
P/N 22943  
Plan #106- 94

Attention: K. Muir, Property Services Division

Re: Discontinuing & Closing those  
portions of the Southern Trans  
Provincial Highway #3

It is requested that the above-mentioned highway right-of-way be discontinued and closed pursuant to the "Highway Act" and according to Ministry regulations.

1. Attached draft notice in duplicate for closure purposes.
2. Interested parties have been so advised.
3. Reason for said closure - as per agreement H.179 attached.
4. Recommended by the Regional Highway Engineer.
5. Reference made to Legal Survey Print No. H-145,  
(no prints necessary; xerox copy enclosed indicating portions to be closed).

  
L. H. Mercier  
Regional Property Negotiator

Recommended by:

  
M. J. O'Connor  
Regional Highway Engineer

JDK/mm

encl.

YALE-LILLOOET ELECTORAL DISTRICT

Discontinuing & Closing those  
portions of the Southern Trans  
Provincial Highway #3

NOTICE is given, pursuant to Section 11 of the  
"Highway Act", that all those portions of:

L.S. 1, 8 & 10, Section 14, Tp. 4, Rg. 25,  
as shown outlined in red;

L.S. 4, Section 13, Tp. 4, Rg. 25, as shown  
outlined in red and green;

L. S. 1, 2, 7, 8, 11, 13 & 14, Section 12,  
Tp. 4, Rg. 25, as shown outlined in red;

L. S. 16, Section 1, Tp. 4, Rg. 24, as shown  
outlined in red;

L. S. 6, 7, 11 & 13 of Section 6, Tp. 4,  
Rg. 24, as shown outlined in green

all West of the 6th Meridian, Yale Division Yale District,  
shown on a plan recorded in the Provincial Ministry of  
Transportation, Communications and Highways, Victoria, B.C.  
under Road Surveys .....(being a copy of a plan  
registered in the Kamloops Land Registry as Plan No. H-145)  
are hereby discontinued and closed.

YALE-LILLOOET ELECTORAL DISTRICT

Discontinuing & Closing those  
portions of the Southern Trans  
Provincial Highway #3

NOTICE is given, pursuant to Section 11 of the  
"Highway Act", that all those portions of:

L.S. 1, 8 & 10, Section 14, Tp. 4, Rg. 25,  
as shown outlined in red;

L.S. 4, Section 13, Tp. 4, Rg. 25, as shown  
outlined in red and green;

L. S. 1, 2, 7, 8, 11, 13 & 14, Section 12,  
Tp. 4, Rg. 25, as shown outlined in red;

L. S. 16, Section 1, Tp. 4, Rg. 24, as shown  
outlined in red;

L. S. 6, 7, 11 & 13 of Section 6, Tp. 4,  
Rg. 24, as shown outlined in green

\* all West of the 6th Meridian, Yale Division Yale District,  
shown on a plan recorded in the Provincial Ministry of  
Transportation, Communications and Highways, Victoria, B.C.  
under Road Surveys .....(being a copy of a plan  
registered in the Kamloops Land Registry as Plan No. H-145)  
are hereby discontinued and closed.

\* L.S. 13, 12, 11, SEC. 6, Twp. 4, Rg. 24  
as shown outlined in red.



Administration Division  
Ministry of Transportation  
and Highways  
VICTORIA

80/6/24

P/N 22943 & R-1996

Attention: Mike Bancroft  
Property Services Division

Re: Establishing Right-of-Way for  
Southern Trans-Provincial Highway  
(Nicolium Creek to W. Bdy. Manning Park)

Attached hereto formal notice in quadruplicate for gazette purposes. It is requested that right-of-way for the above named highway be established pursuant to provisions of the Highway Act for the following reasons:

1. To establish right-of-way which has been settled,  
(and taken from Crown land - copy of letter to Lands Branch attached).
2. Certify that the property owner has been contacted.
3. One sepia and two paper prints of Ministry of Transportation and Highways Construction Branch Drawing R/W - 1 are enclosed.
4. Located in unorganized territory approx. 80 km. east of Hope.
5. Expected date of entry - as soon as possible - detour purposes during construction.  
Copy of H.179 attached.
6. Highway No. 3; project No. 4477.

---

L. H. Meroier  
Regional Property Negotiator

Recommended by:

---

M. J. O'Connor  
Regional Highway Engineer

JDK/mm

encls.

YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right-of-way For

Southern Trans-Provincial Highway,

Nicola Creek to W. Bdy. Manning Park

NOTICE is given, pursuant to Section 6 of the "Highway Act", that all those portions of Legal Subdivisions 11, 12, 13 and 14 of Section 12 together with that portion of Closed Road as shown outlined in green on Plan "H"145; Legal Subdivisions 1 and 7 together with that portion of Closed Road as shown outlined in green on Plan "H"145; Legal Subdivision 2; all Section 14; Legal Subdivision 16, Section 11; all Township 4, Range 25, W6M, YD5D, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys       "(being a copy of Highways Construction Branch Drawing R/W-1), are hereby established as public highway.

MINISTER OF TRANSPORTATION  
AND HIGHWAYS

Ministry of Transportation  
and Highways  
Parliament Buildings  
Victoria, British Columbia

File:

## ASSIGNMENT

## MINISTRY OF HIGHWAYS AND PUBLIC WORKS

To JEAN Date JUNE 25/79  
 File SP. 1090  
 Letter No. \_\_\_\_\_

SUBJECT Route

- |   |   |
|---|---|
| 1. _____ Please investigate and report to this office.    | 9. _____ Please reply direct.                       |
| 2. _____ Please investigate and take action as necessary. | 10. _____ cc to this office.                        |
| 3. _____ Comments, please.                                | 11. _____ Please prepare reply for _____ signature. |
| 4. _____ Recommendations, please.                         | 12. _____ For your attention, please.               |
| 5. _____ Discuss, please.                                 | 13. _____ For your signature, please.               |
| 6. _____ Note and return, please.                         | 14. _____ For your information, please.             |
| 7. _____ Note and forward, please.                        | 15. _____ For your file, please.                    |
| 8. _____ Note and forward to file, please.                |   |

## REMARKS

*Can you get Lloyd Stinson  
 get the particulars on  
 for June 29 Special review  
 agreement w.r.t. to Skipton  
 & subsidiaries.*

*MM*

From \_\_\_\_\_



Government of British Columbia  
Ministry of Transportation, Communications and Highways  
Property Negotiator

H. 179-o  
(Revised Jan. 1977)

File No. P/N 22943  
Electoral District Yale - Lillooet  
Southern Trans Provincial Highway #3  
Project Nicolum Creek to W.Bdy.Manning Park

For and in consideration of the sum of \$1.00, being settlement in full, to be paid upon completion of transfer or gazette Sunshine Valley Developments Ltd  
of 283 E.11th Avenue, Vancouver, B.C., V5T 2C4  
(Address)

hereby agree to convey to Her Majesty The Queen in right of the Province of British Columbia, the following described lands  
Those portions of L.S.7, L.S.1, Section 14, L.S.11, L.S.12 and L.S.13 and L.S.14  
Section 12, Township 4, Range 25, W6M more particularly shown as R/W and outlined  
red on Ministry Location drawing R/W 1 attached.

(for detour purposes only).

Land District, the same to be used for Right-of-Way or other public purposes, free from all encumbrances, the above-mentioned sum to cover all damage to the said land and improvements, if any, consequent upon construction of highway or other public works thereon.

Compensation covers: LAND L.S.7 - 1.481 ac. - Total 9.845/ac partial acquisition  
L.S.1 - 2.318 ac.  
OTHER IMPROVEMENTS: L.S.11- 2.495 ac.  
L.S.12- 0.207 ac.  
L.S.13- 3.339 ac  
L.S.14- 0.005 ac.

Compensation payable forthwith on approval

It is agreed and understood that the above lands are being acquired for detour purposes only and that upon completion of construction Project #4477 will be degazetted and returned to the Transferee free of charge and including any and all improvements situate thereon, and all #3 Hwy traffic removed therefrom, not later than 31 December 1981.

And for the consideration aforesaid the said Claimants

hereby releases HER MAJESTY THE QUEEN, her heirs and successors, from all claims and demands for severance, depreciation, injurious affection, compensation, including compensation under the Highway Act, damages, or other matter or thing arising out of or connected with the expropriation or taking by Her Majesty of the said land.

The vendor acknowledges that no verbal commitments have been made to him by any agent of said ministry.

No other party has an equity in the lands aforesaid, other than as set forth hereunder and compensation is therefore payable only as stipulated herein. This agreement is subject to the approval of the Deputy Minister of Transportation, Communications and Highways.

Given under my hand and seal at

British Columbia this

day of

1980

WITNESS

SIGNATURE

Title owner: Sunshine Valley Developments Ltd

To whom compensation payable Sunshine Valley Developments Ltd

Encumbrances Mortgage in favour of William Kenneth Rosenberry

Clear Title

Total Area of Parcel 1045 ac more or less

Taxes and or adjustment shall be the responsibility of owners

Government of British Columbia  
Ministry of Highways and Public Works  
Property Negotiator

H. 179  
(Revised Jan. 1977)

Electoral District Yale-Lillooet

Project T.C.H. Nicolum Creek - Hope Slide

For and in consideration of the sum of \$76 830.00, being settlement in full,  
to be paid upon completion of transfer or gazette Sunshine Valley Developments Ltd.

283 East 11th Avenue of Vancouver, B. C. V5T 2C4  
(Address)

hereby agree to convey to Her Majesty The Queen in right of the Province of British  
Columbia, the following described lands "See Schedule "A" attached."

Land District, the same to be used for Right-of-Way or other  
public purposes, free from all encumbrances, the above-mentioned sum to cover all damage  
to the said land and improvements, if any, consequent upon construction of highway or  
other public works thereon.

Compensation covers: LAND 62.98/acre<sup>+</sup> partial acquisition

OTHER <del>XXXXXXXXXXXX</del>	<u>Resumable 4.20 acre<sup>+</sup></u>	<u>Existing R/W to be transferred</u>
<u>Old R/W to be transferred Plan H 145</u>		<u>after construction 19.74 acre<sup>+</sup></u>
	<u>31.46 acre<sup>+</sup></u>	
<u>Compensable area</u>	<u>21.37 acre<sup>+</sup></u>	

Compensation payable as follows:  
\$76 830.00 forthwith on approval.

For covenants and commitments see Schedule "A" attached.

And for the consideration aforesaid the said claimants

hereby releases HER MAJESTY THE QUEEN, her heirs and successors, from all claims and  
demands for severance, depreciation, injurious affection, compensation, including com-  
pensation under The Highway Act, damages, or other matter or thing arising out of or  
connected with the expropriation or taking by Her Majesty of the said land.

The vendor acknowledges that no verbal commitments have been made to him by any agent  
of said ministry.

No other party has an equity in the lands aforesaid, other than as set forth hereunder  
and compensation is therefore payable only as stipulated herein. This agreement is  
subject to the approval of the Deputy Minister of Highways and Public Works.

Given under my hand and seal at Vancouver British Columbia this 26<sup>th</sup>  
day of June 1978

WITNESS [Signature] SIGNATURE [Signature]

Title owner: Sunshine Valley Developments Ltd. President

To whom compensation payable Sunshine Valley Developments Ltd.

S22

Encumbrances

S22

Total Area of Parcel 1045 acre<sup>+</sup>

Taxes shall be the responsibility of owners.



- 1 -

Forming part of agreement with

Name : SUNSHINE VALLEY DEVELOPMENTS LTD.

Description: T. C. H. NICOLUM CREEK - HOPE SLIDE

Those portions of L. S. 16, Section 31 - L. S. 13, Section 32, Twp. 3, Rge. 24, W6M, Y.D.Y.D. - L. S. 12, 11, 7, 6, 5, 2 & 1, Section 6, Twp. 4, Rge. 24, W6M Y.D.Y.D. - L. S. 16, 15 & 9, Section 1, Twp. 4, Rge. 25, W6M, Y.D.Y.D. - L. S. 12, 6, 3 & 2, Section 12, Twp. 4, Rge. 25, W6M, Y.D.Y.D. - L.S. 10, 7 & 1, Section 14, Twp. 4, Rge. 25, W6M, Y.D.Y.D. particularly shown as R/W and Severances coloured red and green on Ministry of Highways Location

Drawings R2-22-5 to R2-22-12 incl (#15-94)

## COVENANTS &amp; COMMITMENTS AS FOLLOWS:

The Ministry agrees to reconstruct access to the viewpoint of the Hope Slide at approximately Station 138 left in L. S. 7 - the access to Cedar Village @ approx. Station 248 left and right L. S. 9 including acceleration and deceleration lanes on both sides of new Right-of-Way.

The Ministry further agrees to supply and install a boat underpass @ approx. Station 231 left and right L. S. 15 the cost of which has been apportioned and deducted from the compensation payable. An equestrian - pedestrian underpass @ approx. Station 256 left and right and to supply and erect a standard highway wire fence on both the north and south sides of the new R/W from Station approx. 220 to 307. Permission is hereby granted by the owners allowing the Ministry or their assigns to enter upon their property for the purpose of completing the aforementioned works and also to construct necessary creek diversions including one @ fourteen-mile creek @ approx. Station 288 left in L. S. 7.

*[Signature]*  
178/6/26

*[Signature]*



- 2 -

Forming part of agreement with

Name : SUNSHINE VALLEY DEVELOPMENTS LTD.

Description: T. C. H. NOCOLUM CREEK - HOPE SLIDE

It is agreed and understood that the Ministry of Highways intends to close  
and transfer those portions of old highway R/W Plan H 145 which cross private  
lands being the property of the owners herein and will also close and transfer  
those portions of existing travelled R/W not required for construction  
purposes after the construction has been completed. Adjustments between  
areas required and those to be transferred have been made and deducted from  
the compensation payable.

The aforementioned closures and transfers shall be subject to the approval  
of the Lieutenant-Governor-In-Council.

The Ministry of Highways agrees to expedite construction of this project  
through claimant's property and complete same by 1980.

The owners herein hereby agree to provide or allow for the necessary  
easements of the B. C. Hydro telephone facilities along the old or existing  
R/W's as required.

There shall be no access onto or crossings over the controlled access  
highway except at the designated intersections.

No further commitments.

*[Signature]*  
78/6/26

*[Signature]*



# COMPENSATION IN TERMS OF THE HIGHWAY ACT

(SECTION 14)

H.Q. Project P - 0113 Plan No. RW 1 P/N File 22943  
Highway District No. 15 Electoral District Yale-Lillooet Engineering District  
Name and details of highway or incidental works Southern Trans Provincial Highway 3  
Nicolium Creek to W.Bdy. Manning Park

Municipality \_\_\_\_\_ Unorganized Yes  
Description of property Those portions of L.S.7, L.S.1, Section 14, L.S. 11, L.S.12,  
L.S.13 and L.S.14, Section 12, Township 4, Range 25, W6M, more particularly  
shown as R/W and outlined red on Ministry Location Drawing R/W1 attached.  
(For detour purposes only)  
Name of registered owner Sunshine Valley Developments Ltd

Is title encumbered? No If yes, give details of encumbrance \_\_\_\_\_

Name, title, etc., of party to whom compensation is recommended to be paid Sunshine Valley Developments Ltd.

## DETAILS OF COMPENSATION RECOMMENDED TO BE PAID PURSUANT TO SEC. 16 OF THE HIGHWAY ACT

### A. For Improvements on the Lands Taken

(Subsec. (1) (a).) "Improvements on the land so taken, that is to say, everything constructed on or annexed to the soil by the hand of man."

Roads: Area,

Buildings: Area and description,

Fences: Length,

Clearing: Area,

Planting: Area,

Cultivation: Area,

Other improvements (to be stated) \_\_\_\_\_

Remarks: \_\_\_\_\_

COMPENSATION RECOMMENDED

A.

### B. For Lands Taken (Not Subdivided Lands)

(Subsec. (1) (b).) "Lands which were originally granted to some person by the Crown, either in right of the Province or Canada, and by the taking of which the total area taken for the purpose of highways from the lands comprised in the original Crown grant is found to exceed one-twentieth of the total area of the lands comprised in the Crown grant, and then only for the area in excess of one-twentieth of that total area."

Area of Crown grant - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)

Total area taken for road purposes { Old - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)  
New - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)

Deduct one-twentieth of area of Crown grant - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)

Area for which compensation is payable - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)

The land is valued at \$ \_\_\_\_\_ per hectare (acre).

COMPENSATION RECOMMENDED \$ \_\_\_\_\_ B.

I hereby certify that this statement is in accordance with the provisions of section 16 of the Highway Act.

Date 80/6/27, 19 \_\_\_\_\_

J.D. KILLER

(Property Negotiator)

Date 27 JUN 1980, 19 \_\_\_\_\_

(Regional Property Negotiator)

I hereby recommend that compensation be paid accordingly

Date 80/6/27, 19 \_\_\_\_\_

(Regional Highway Engineer)

APPROVED:

\_\_\_\_\_, 19 \_\_\_\_\_

(Director or Assistant Director of Property Services)

\_\_\_\_\_, 19 \_\_\_\_\_

(Dealt with)

(Deputy Minister or Assistant Deputy Minister)



**C. For Lands Taken (Subdivided Lands Only)**

(Subsec. (1) (b).) "But, where the lands comprised in the Crown grant have been subdivided into parcels by any registered conveyance or plan of subdivision, the area of land which may be so taken from any parcel without payment of compensation shall not exceed one-twentieth of the area of that parcel, and where the lands are being taken from two or more of the parcels at the same time, the total area to be so taken without the payment of compensation shall be apportioned among those parcels on the basis of their respective areas."

Details of registered subdivision plan, parcel, lot, etc. L.S.7, L.S.1, Sec.14, L.S. 11,12,13 and 14, Sec.12  
Two.4, Rge.25, W6M

Total area of parcel - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)

Total area taken for road purposes { Old - See attached ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)  
New - schedule ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)

Deduct one-twentieth area of parcel - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)

Area for which compensation is payable - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)

The land is valued at \$ N/A per hectare (acre).

Token compensation

COMPENSATION RECOMMENDED \$ \$1.00 C.

**D. For Materials Taken from Improved Lands**

(Subsec. (4).) "Where in respect of gravel, sand, stone, timber, or other materials taken by Crown for construction, maintenance, or repair of any highway or bridge, the provisions of any Statute or the reservations contained in any Crown grant permit of the same being so taken without payment of compensation, and where the same is taken from Improved Lands, the Minister may, notwithstanding the provisions or reservations aforesaid, pay reasonable compensation therefor."

CLASS OF MATERIAL TAKEN	QUANTITY	UNIT RATE	AMOUNT

TOTAL COMPENSATION RECOMMENDED \$   D.

**E. Deduction for Increased Capital Value due to Construction**

(Subsec. (3).) "In determining the compensation payable to any owner in respect of any land entered upon and taken possession of under this Part, there shall be taken into consideration the increased value, beyond the increased value common to all lands in the locality, that will be given to the remaining lands of the owner through which the highway will pass, by reason of the passage of the highway through the same or by reason of the construction of the highway or of works incidental thereto, and the increased value that will be so given shall be set off against the compensation otherwise payable to that owner under this section."

Details \_\_\_\_\_

TOTAL DEDUCTIONS \$   E.

**Remarks:**

(NOTE—Give details and approximate value of any other compensation arranged for and recommended, such as fencing or transfer to adjacent owner of land comprised in old road to be closed, or other matters.)

Compensation payable forthwith on approval

It is agreed and understood that the above lands are being acquired for detour purposes only and that upon completion of construction Project 4477 will be degazetted and returned to the Transferee free of charge and including any and all improvements situate thereon and all Highway #3 traffic removed therefrom not later than 31st December 1981.

(Date specified for vacation has been approved by Contractor, Regional Highway Engineer and Regional Construction Engineer).

Agreement Form signed 80/6/25

**SUMMARY OF AMOUNT OF COMPENSATION RECOMMENDED TO BE PAID**

A. For improvements on lands taken - - - - - \$   A.  
OR { B. For lands taken (not subdivided lands) - - - - - \$   B.  
C. For lands taken (subdivided lands only) - - - - - \$ 1.00 C.  
D. For materials taken from improved lands - - - - - \$ 1.00 D.  
Gross total amount of compensation recommended - - - - - \$ 1.00  
E. Deduction for increased value - - - - - \$   E.  
NET TOTAL OF COMPENSATION RECOMMENDED TO BE PAID - - - - - \$ 1.00  
Cheques \$1.00



ATTACHMENT TO H.120 IN THE NAME OF SUNSHINE VALLEY DEVELOPMENTS LTD

P/N 22943

	<u>L.S.7</u> 40 Ac.	<u>L.S.1</u> 40 Ac.	<u>L.S.11</u> 40 Ac.	<u>L.S.12</u> 40 Ac.	<u>L.S.13</u> 40 Ac.	<u>L.S.14</u> 40 Ac.
Total area of Parcel						
Total area taken for road ) Old purposes ) New	6.65 " 1.481 "	3.76 Ac. 2.318 Ac.	3.67 Ac 2.495 Ac	4.98 Ac 0.207 Ac	4.93 Ac. 3.339 Ac	1.25 Ac 0.005 "
Deduct 1/20th area of parcel	2.0 "	2.0 Ac.	2.0 Ac	2.0 Ac	2.0 Ac	2.0 "
Area for which compensation is payable.	1.481 "	2.318 Ac.	2.495 Ac.	0.207 Ac.	3.339 Ac.	Nil

MEMO

MINISTRY OF TRANSPORTATION, ~~COMMUNICATIONS~~ AND HIGHWAYS

To: Regional Engineer \_\_\_\_\_ P.N. File 22943  
District Highway Manager \_\_\_\_\_ Your File \_\_\_\_\_  
Regional Construction Engineer Burnaby, B.C. Date 80/6/27


The following terms have been agreed upon in connection with:

Project No. Southern Trans Provincial Highway 3  
Neechum Creek to W.Bdy. Manning Park  
Owner Sunshine Valley Developments Ltd

Legal description Those portions of L.S.7, L.S.1 Sec.14, L.S. 11, L.S.12, L.S.13 and L.S.14  
Section 12, Township 4, Range 25, W6M; more particularly shown as R/W  
and outlined red on Ministry Location Drawing RW1.

Terms: It is agreed and understood that the above lands are being acquired  
for detour purposes only and that upon completion of construction  
Project #4477 will be degazetted and returned to the Transferee free  
of charge and including any and all improvements situate thereon, and  
all #3 Highway traffic removed therefrom, not later than 31st  
December 1981.

	Yes	No
Right-of-way has been acquired _____	<input type="checkbox"/>	<input type="checkbox"/>
Right-of-way has been gazetted _____	<input type="checkbox"/>	<input type="checkbox"/>
It is in order to enter for construction _____	<input type="checkbox"/>	<input type="checkbox"/>

  
Property Negotiator

## GOVERNMENT OF BRITISH COLUMBIA - MINISTRY OF HIGHWAYS AND PUBLIC WORKS

## COMPENSATION IN TERMS OF THE HIGHWAY ACT

(SECTION 16)

H.Q. Project 3523 Plan No. R2-22-5 to R2-22-12 P/N File 22943  
 Highway District No. 15 Electoral District Yale-Lillooet Engineering District  
 Name and details of highway or incidental works H.Q. Project 3523  
T. C. H. Nicillum Creek - Hope Slide

Municipality \_\_\_\_\_ Unorganized Fraser-Cheam Regional District  
 Description of property See Schedule 'A' attached.

Name of registered owner Sunshine Valley Developments Ltd.

Is title encumbered? yes If yes, give details of encumbrance

S22

Name, title, etc., of party to whom compensation is recommended to be paid Sunshine Valley Developments Ltd.  
 S22

## DETAILS OF COMPENSATION RECOMMENDED TO BE PAID PURSUANT TO SEC. 16 OF THE HIGHWAY ACT

## A. For Improvements on the Lands Taken

(Subsec. (1) (a).) "Improvements on the land so taken, that is to say, everything constructed on or annexed to the soil by the hand of man."

Roads: Area,Buildings: Area and description,Fences: Length,Clearing: Area,Planting: Area,Cultivation: Area,

Other improvements (to be stated) \_\_\_\_\_

Remarks:

COMPENSATION RECOMMENDED

A.

## B. For Lands Taken (Not Subdivided Lands)

(Subsec. (1) (b).) "Lands which were originally granted to some person by the Crown, either in right of the Province or Canada, and by the taking of which the total area taken for the purpose of highways from the lands comprised in the original Crown grant is found to exceed one-twentieth of the total area of the lands comprised in the Crown grant, and then only for the area in excess of one-twentieth of that total area."

Area of Crown grant - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)

Total area taken for road purposes { Old - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)  
 New - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)

Deduct one-twentieth of area of Crown grant - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)

Area for which compensation is payable - - - - - ha (ac.) - - - - - ha (ac.) - - - - - ha (ac.)

The land is valued at \$ \_\_\_\_\_ per hectare (acre).

COMPENSATION RECOMMENDED \$

B.

I hereby certify that this statement is in accordance with the provisions of section 16 of the Highway Act

Date June 28th, 19 78

30 JUN 1978

Date \_\_\_\_\_, 19 \_\_\_\_\_

I hereby recommend that compensation be paid accordingly

Date July 4, 19 78

APPROVED:

Date \_\_\_\_\_, 19 \_\_\_\_\_

Date \_\_\_\_\_, 19 \_\_\_\_\_

(Dealt with)

(Director or Assistant Director of Property Services)

(Deputy Minister or Assistant Deputy Minister)

(OVER)



C. For Lands Taken (Subdivided Lands Only)

(Subsec. (1) (b).) "But, where the lands comprised in the Crown grant have been subdivided into parcels by any registered conveyance or plan of subdivision, the area of land which may be so taken from any parcel without payment of compensation shall not exceed one-twentieth of the area of that parcel, and where the lands are being taken from two or more of the parcels at the same time, the total area to be so taken without the payment of compensation shall be apportioned among those parcels on the basis of their respective areas."

See Schedule 'A' attached.

Details of registered subdivision plan, parcel, lot, etc.

Total area of parcel	- - - - -	ha (ac.)	ha (ac.)	ha (ac.)
Total area taken for road purposes	{ Old - - - - -	ha (ac.)	ha (ac.)	ha (ac.)
	{ New - - - - -	ha (ac.)	ha (ac.)	ha (ac.)
Deduct one-twentieth area of parcel	- - - - -	ha (ac.)	ha (ac.)	ha (ac.)
Area for which compensation is payable	76 830.00	ha (ac.)	ha (ac.)	ha (ac.)
The land is valued at \$	per hectare (acre).		76 830.00	

COMPENSATION RECOMMENDED \$ C.

D. For Materials Taken from Improved Lands

(Subsec. (4).) "Where in respect of gravel, sand, stone, timber, or other materials taken by Crown for construction, maintenance, or repair of any highway or bridge, the provisions of any Statute or the reservations contained in any Crown grant permit of the same being so taken without payment of compensation, and where the same is taken from Improved Lands, the Minister may, notwithstanding the provisions or reservations aforesaid, pay reasonable compensation therefor."

CLASS OF MATERIAL TAKEN	QUANTITY	UNIT RATE	AMOUNT
-------------------------	----------	-----------	--------

TOTAL COMPENSATION RECOMMENDED \$ D.

E. Deduction for Increased Capital Value due to Construction

(Subsec. (3).) "In determining the compensation payable to any owner in respect of any land entered upon and taken possession of under this Part, there shall be taken into consideration the increased value, beyond the increased value common to all lands in the locality, that will be given to the remaining lands of the owner through which the highway will pass, by reason of the passage of the highway through the same or by reason of the construction of the highway or of works incidental thereto, and the increased value that will be so given shall be set off against the compensation otherwise payable to that owner under this section."

Details

TOTAL DEDUCTIONS \$ E.

Remarks:

(NOTE—Give details and approximate value of any other compensation arranged for and recommended, such as fencing or transfer to adjacent owner of land comprised in old road to be closed, or other matters.)

Compensation payable as follows:

\$76 830.00 forthwith on approval.

For covenants and commitments see Schedule 'A' attached.

June 26th, 1978

Agreement Form signed

SUMMARY OF AMOUNT OF COMPENSATION RECOMMENDED TO BE PAID

A. For improvements on lands taken	- - - - -	\$	A.
OR B. For lands taken (not subdivided lands)	- - - - -	\$ 76 830.00	B.
C. For lands taken (subdivided lands only)	- - - - -	\$	C.
D. For materials taken from improved lands	- - - - -	\$ 76 830.00	D.
Gross total amount of compensation recommended	- - - - -	\$	
E. Deduction for increased value	- - - - -	\$ 76 830.00	E.
NET TOTAL OF COMPENSATION RECOMMENDED TO BE PAID	- - - - -	\$	
1 cheque - \$ 76 830.00			
Cheques			

struction." If the bidder does not already have a copy of these specifications, one may be obtained from the Provincial Government Plan Viewing Room, Vancouver, or from the undersigned for the sum of \$10 (cheque or money order made payable to the Minister of Finance), which is not refundable. All copies purchased are registered and amendments are forwarded when issued.

Each tender must be accompanied by a bid bond or a certified cheque in an amount equal to 10 per cent of the tender price. A performance bond and a labour and material payment bond, each in the amount of 50 per cent of the tendered sum, in a form acceptable to the Minister of Finance, must then be provided by the successful bidder upon receipt of notice of acceptance of tender. If a bid bond is supplied, this bond shall be forfeited if the tenderer declines to enter into a contract if called upon to do so. In the event that the successful bidder supports his tender with a certified cheque, but fails to produce the required performance bond and labour and material payment bond within a period of 14 days from the date of issuance of notice that his tender is accepted, the Minister of Highways shall have the right to forfeit the tender deposit in its full amount or to the extent of the difference in cost between the bidder's tender and the accepted tender, whichever is the lesser.

Tenders must be made out on the forms supplied and enclosed in the envelope furnished.

The tender submitted is irrevocable for 60 days and may not be altered, amended, or withdrawn for any cause without the written permission of the Minister.

Tenderers are advised that all bidders will be required to satisfy the Minister of Highways, in writing, that they have the necessary qualifications to successfully perform the work. It is preferred that tenderers submit with their tenders a list stating the type, capacity, and present location (so that inspection can be made if necessary) of the equipment in their possession or at their disposal which is immediately available for the carrying-out of the work within the time limit specified without in any way curtailing or delaying any Departmental project that may be under contract at this date.

No tender will be accepted or considered which contains an escalator clause or any other qualifying conditions, and the lowest or any tender will not necessarily be accepted.

It is preferred that tenderers submit with their tenders a schedule of proposed construction operations showing estimated monthly progress for each phase of the construction work.

H. T. MIARD  
Deputy Minister

Department of Highways,  
Parliament Buildings,  
Victoria, B.C., May 1973.

File 28 jy5-4610

#### YALE LILLOOET ELECTORAL DISTRICT

Establishing Additional Right-of-way for Southern Trans-Canada Highway, Hope-Princeton Section.

NOTICE is given, pursuant to section 8 of the *Highway Act*, that all those portions of Legal Subdivisions 2 and 7 of Section 14, Township 4, Range 25, W6M, as shown outlined in red on a plan recorded in the Provincial Department of Highways, Victoria, B.C., under "Road

Surveys 3329-1" (being a copy of Highway Location Drawing P2-PA-5, sheet 5), are hereby established as public highway.

G. R. LEA  
Minister of Highways

Department of Highways,  
Parliament Buildings,  
Victoria, B.C., June 28, 1973.

File 292/277767 jy5-4614

#### NOTICE TO CONTRACTORS

##### Mackenzie Electoral District

Project T.F. 164—Langdale Ferry Terminal. Contract 1: Parking and Compound Area Revisions.

CLOSING DATE for receipt of tenders has been extended to 2 p.m. (Pacific Daylight Saving time) on Thursday the 5th day of July 1973.

H. T. MIARD  
Deputy Minister

Department of Highways,  
Parliament Buildings,  
Victoria, B.C., June 26, 1973.

File 892-16 jy5-4603

#### NOTICE TO CONTRACTORS

##### South Peace River Electoral District

Bridge Project 790—Pouce Coupe River Bridge, Dawson Creek-Spirit River Highway, Contract 3: Deck.

CLOSING DATE for tenders is revised to read "2 p.m. (Pacific Daylight Saving time) on Tuesday the 17th day of July 1973."

H. T. MIARD  
Deputy Minister

Department of Highways,  
Parliament Buildings,  
Victoria, B.C., June 29, 1973.

File 4422 jy5-4612

#### NOTICE TO CONTRACTORS

##### Yale-Lillooet Electoral District

Project 2486—Trans-Canada Highway 1, Highway 9 to Wahleach Power Station, Inventory Mile 31.77, Mount Lehman-Bridal Falls Subdivision, to Mile 6.06, Bridal Falls-Hope Subdivision, Station 845+00 to Station 889+99.68 Bk., Station 0+00 to Station 296+50.00 (6.45 Miles).

CLOSING DATE for receipt of tenders extended to 2 p.m. (Pacific Daylight Saving time) on Thursday the 5th day of July 1973.

H. T. MIARD  
Deputy Minister

Department of Highways,  
Parliament Buildings,  
Victoria, B.C., June 26, 1973.

File 3416 jy5-4606

#### FORT GEORGE ELECTORAL DISTRICT

Establishing Right-of-way for Yellowhead Highway 16, 20th Avenue to Parkridge Subdivision.

NOTICE is given, pursuant to section 8 of the *Highway Act*, that all those portions of the northeast quarter of District Lot 1599 (except Plans 10790, 11601, 13535, 13589, 13820, 14340, 18049, and 20113); the east half of the northwest

quarter of District Lot 1599 (except Plans 10790, 13527, 13535, and 14373); and Lot 11, Plan 10790, District Lot 1599, all in Cariboo District, as shown outlined in red on a plan recorded in the Provincial Department of Highways, Victoria, B.C., under "Road Surveys 4307" (being a copy of Highway Location Drawing R4-7-12 R.W. and R4-7-13 R.W.), are hereby established as public highway.

G. R. LEA  
Minister of Highways

Department of Highways,  
Parliament Buildings,  
Victoria, B.C., July 5, 1973.

File 5642/278010 jy5-4614

#### NOTICE TO CONTRACTORS

##### Prince Rupert Electoral District

Bridge Project 853—Cleaning and Painting Galloway Rapids Bridge

CLOSING DATE for receipt of tenders has been extended to 2 p.m. (Pacific Daylight Saving time) on Wednesday the 4th day of July 1973.

H. T. MIARD  
Deputy Minister

Department of Highways,  
Parliament Buildings,  
Victoria, B.C., June 26, 1973.

File 3588-8 jy5-4601

#### NOTICE TO CONTRACTORS

##### Comox Electoral District

Bridge Project 816—Tsable River Bridge, Island Highway, Contract 1: General

CLOSING DATE for receipt of tenders has been extended to 2 p.m. (Pacific Daylight Saving time) on Wednesday the 4th day of July 1973.

H. T. MIARD  
Deputy Minister

Department of Highways,  
Parliament Buildings,  
Victoria, B.C., June 26, 1973.

File 571 jy5-4602

#### NOTICE TO CONTRACTORS

##### Boundary-Similkameen Electoral District

Project 2508—Contract 2: Crushed Granular Surfacing in Stockpile, Keremeos

CLOSING DATE for receipt of tenders has been extended to 2 p.m. (Pacific Daylight Saving time) on Thursday the 5th day of July 1973.

H. T. MIARD  
Deputy Minister

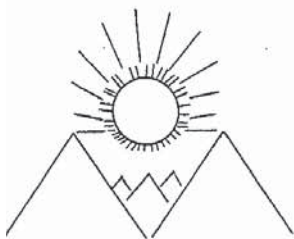
Department of Highways,  
Parliament Buildings,  
Victoria, B.C., June 26, 1973.

File 624-2 jy5-4605

#### CARIBOO ELECTORAL DISTRICT

Establishing Additional Right-of-way for Cariboo Highway 97, 100 Mile North

NOTICE is given, pursuant to section 8 of the *Highway Act*, that all those portions of District Lots 1085, 2830; 190, 191, 1084, 3599, 3595, 3596, 76; 783, 3552, 3553, 3554 (except Plan 19245), 200, 3558, 3560, 358 (except Parcel A, Plan B7138, and Plans RA42012-15820



# Sunshine Valley Developments Ltd.

July 18, 1980.

RECEIVED  
REGIONAL PROPERTY NEGOTIATOR

(JUL 17 1980)

ANSWERED.....  
NOTED.....  
DATE.....

Ministry of Highways,  
7818 Sixth Street,  
Burnaby, B.C.  
V3N 4N8, Phone: 525-0961

Attention: Mr. Jim Killer - Highway Negotiator

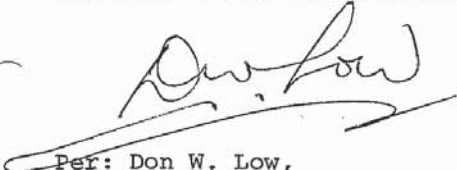
Dear Sirs:

Enclosed herewith is a copy of Gazette notice on page 312 of the British Columbia Gazette, dated February 22, 1979, showing the "Establishing Right-of-way for Sunshine Valley Road".

We would appreciate having full particulars of what this Gazette note covers. Many thanks.

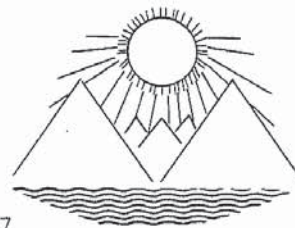
Sincerely,

SUNSHINE VALLEY DEVELOPMENTS LTD.,

  
Per: Don W. Low,  
President

DWL/b

Encl.



283 East 11th Avenue, Vancouver, Canada V5T 2C4 (604) 873-3377



W6M, KDYD, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation, Communications and Highways, Victoria, B.C., under "Road Surveys 5920" (being a copy of Highway District Plan D-146-2), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation,  
Communications and  
Highways  
Ministry of Transportation,  
Communications and Highways,  
Parliament Buildings,  
Victoria, B.C., February 15, 1979.  
File 21-20-02/337019 fe22-2281

#### SURREY ELECTORAL DISTRICT

Project 3983—Reconstruction: Fraser Highway 1A to 62nd Avenue on King George V Highway 99A

CLOSING DATE for receipt of tenders is extended to Tuesday, February 20, 1979, at 2 p.m.

R. G. HARVEY  
Deputy Minister  
February 12, 1979.  
File 14-01-54 fe22-2280

#### SURREY ELECTORAL DISTRICT

Establishing Right-of-way for Fraser Highway Vicinity of 160th Street (Pike Road)

NOTICE is given, pursuant to section 8 of the Highway Act, that all those portions of Parcel A (Exp. Plan 10962) of Lots 39 and 41 of Blocks 2 and 7 of the northeast quarter of Section 26, Plan 2824; Parcel A (Exp. Plan 10841) of Lot 40 of the northeast quarter of Section 26, Plan 2824; Lot 40 of the northeast quarter of Section 26, Plan 2824 (except Parcel A (Exp. Plan 10841)); Lot 24 of the northwest quarter of Section 25, Plan 26463; Lot J of Parcel B of the northwest quarter of Section 25, Plan 19257; Lot 5 of the northwest quarter of Section 25, Plan 9175, all Township 2, N.W.D., as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation, Communications and Highways, Victoria, B.C., under "Road Surveys 5921" (being a copy of Highway District Plan 30-14 R/W), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation,  
Communications and  
Highways  
Ministry of Transportation,  
Communications and Highways,  
Parliament Buildings,  
Victoria, B.C., February 15, 1979.  
File 14-20-01/337268 fe22-2282

#### MOTOR-VEHICLE ACT

PURSUANT to section 140 (3a) of the Motor-vehicle Act, the following area is defined as a speed zone area:  
1923 Hendrix Lake Townsite—The land lying within Plan 14605, Lot 12046, Cariboo District.

ALEX. V. FRASER  
Minister of Transportation,  
Communications and  
Highways  
Ministry of Transportation,  
Communications and Highways,  
Parliament Buildings,  
Victoria, B.C., February 15, 1979.  
File 70-00-24 fe22-2282

#### YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right-of-way for Trans-Canada Highway, Vicinity of Dogwood Valley

NOTICE is given, pursuant to section 8 of the Highway Act, that all that part of Lot 1, Section 22, Township 6, Range 26, W6M, Plan 3137, YDYD (except Plan 13685), as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation, Communications and Highways, Victoria, B.C., under "Road Surveys 5917" (being a copy of Highway District Plan 174-2), is hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation,  
Communications and  
Highways  
Ministry of Transportation,  
Communications and Highways,  
Parliament Buildings,  
Victoria, B.C., February 15, 1979.  
File 15-20-03/336735 fe22-2282

#### YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right-of-way for Kamuse Road 241

NOTICE is given, pursuant to section 8 of the Highway Act, that all that part of District Lot 933, YDYD (except Plan 10227), as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation, Communications and Highways, Victoria, B.C., under "Road Surveys 5916" (being a copy of Highway District Plan 54-12), is hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation,  
Communications and  
Highways  
Ministry of Transportation,  
Communications and Highways,  
Parliament Buildings,  
Victoria, B.C., February 15, 1979.  
File 25-20-02/336736 fe22-2282

#### YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right-of-way for Sunshine Valley Road

NOTICE is given, pursuant to section 8 of the Highway Act, that all those portions of District Lot 1022, Plan B1111; District Lot 1022 (except Parcel B, Plan B1111 and Plan 23138); and the southeast quarter of Section 1, Township 14, Range 23, W6M, lying north of the right bank of the Nicola River (except plan attached to DD 10725 and Plan 11246), all Kamloops Division of Yale District, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation, Communications and Highways, Victoria, B.C., under "Road Surveys 5924" (being a copy of Highway District Plan 39-12), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation,  
Communications and  
Highways  
Ministry of Transportation,  
Communications and Highways,  
Parliament Buildings,  
Victoria, B.C., February 15, 1979.  
File 25-01-14/337265 fe22-2282

#### LEGISLATIVE ASSEMBLY

##### PRIVATE BILLS

Excerpts From Standing Orders Relating to Private Bills

97. No petition for any Private Bill shall be received by the House after the first 10 days of each Session, nor may any Private Bill be presented to the House after the first three weeks of each Session, nor may any report of any Standing or Select Committee upon a Private Bill be received after the first four weeks of each Session, and no motion for the suspension or modification of this Standing Order shall be entertained by the House until the same has been reported on by the Committee on Standing Orders, or after reference made thereof at a previous sitting of the House to the Standing Committees charged with the consideration of Private Bills, who shall report thereon to the House. And if this rule shall be suspended or modified as aforesaid, the promoters of any Private Bill which is presented after the time hereinbefore limited, or for which the petition has been received after the time hereinbefore limited, shall in either case pay double the fees required in respect of such Bill by this rule, unless the House shall order to the contrary.

98. (1) Any person seeking to obtain any Private Bill shall deposit with the Clerk of the House, eight clear days before the opening of the Session,

- (a) a printed copy of such Bill;
- (b) a copy of the petition to be presented to the House;
- (c) copies of the notices published, with publication proved by a statutory declaration or affidavit to the satisfaction of the Clerk of the House.

(2) At the time of depositing the Bill and petition the applicant shall also pay to the Clerk of the House the sum of \$300.

(3) If a copy of the Bill, petition, and notices proved as aforesaid shall not have been so deposited in the hands of the Clerk of the House at least eight clear days before the opening of the Session or if the petition has not been presented to the House within the first 10 days of the Session, the amount to be paid to the Clerk shall be \$600. If the Bill shall not pass second reading, one-half of the fees shall be returned.

100. (1) All applications to the Legislative Assembly for Private Bills of any nature whatsoever shall require the publication of a notice clearly and distinctly specifying the nature and object of the application, and when the application refers to any proposed work, indicating sufficiently the location of the work, to be signed by or on behalf of the applicant.

(2) Such notice shall be published in *The British Columbia Gazette* and once a week in one leading daily newspaper freely circulating in all parts of the Province. The notice in such newspaper to appear on Wednesday of each week.

(3) When the proposed Bill is of such a nature that it may particularly affect one or more electoral districts, the notice shall furthermore be published once a week if possible in some newspaper in every electoral district affected, or if there be no newspaper published therein, then in a newspaper published in the electoral district nearest thereto.



## YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right of Way for Southern Trans-Provincial Highway

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all those portions of District Lots 229 and 1195 (except those portions thereof included within the boundaries of District Lots 901 and 627), YDYL, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 6590" (being a copy of Highway District Plan 16-80-01), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation  
and Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., June 26, 1980.  
File 16-20-00/349539 jy24—3988

## YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right of Way for Southern Trans-Provincial Highway, Nicolum Creek to West Boundary of Manning Park

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all those portions of Legal Subdivisions 11, 12, 13 and 14 of Section 12 together with that portion of "Closed Road" as shown outlined in green on Plan H145; Legal Subdivisions 1 and 7 together with that portion of "Closed Road" as shown outlined in green on Plan H145; Legal Subdivision 2; all Section 14; Legal Subdivision 16, Section 11; all Township 4, Range 25, W6M, YDYL, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 6622" (being a copy of Highway Construction Branch Drawing R/W-1), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation  
and Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., July 17, 1980.  
File 16-20-00/340877 jy24—3988

## COMOX ELECTORAL DISTRICT

Establishing Right of Way for Guthrie Road 151, Vicinity of Comox

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all those portions of the southeast quarter, and the west half (except Plan 429RW), Section 71, Comox District, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 6597" (being a copy of Highway District Plan 64-7-76), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation  
and Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., June 26, 1980.  
File 64-20-33/340293 jy24—3986

## YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right of Way for Southern Trans-Provincial Highway

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all those portions of District Lots 229 and 1195 (except those portions thereof included within the boundaries of District Lots 901 and 672), YDYL, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 6591" (being a copy of Highway District Plan 16-79-35), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation  
and Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., June 26, 1980.  
File 16-20-00/349540 jy24—3988

## KAMLOOPS ELECTORAL DISTRICT

Discontinuing and Closing Road Allowance Within DL 1483, KDYL

NOTICE is given, pursuant to section 9 of the *Highway Act*, that all that part of road allowance within District Lot 1483, KDYL, as shown outlined in green on a plan of survey recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 6593," is hereby discontinued and closed.

The portion of road allowance so discontinued and closed is indicated by solid and broken lines and the words "Closed Road" on a "Reference Plan of Lot 4, Plan 29023 and Closed Road, District Lot 1483, Kamloops Division Yale District", prepared by R. B. Stothers, B.C.L.S., the survey for which was completed on February 6, 1980, to be deposited in the Kamloops Land Title Office.

ALEX. V. FRASER  
Minister of Transportation  
and Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., June 26, 1980.  
File 21-01-78/340473 jy24—3986

## YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right of Way for Meadow Creek Road 178

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all those portions of unsurveyed Crown land adjacent to District Lots 6022 and 5967, KDYL, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 6594" (being a copy of Highway District Plan 106-5), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation  
and Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., June 26, 1980.  
File 25-20-09/340298 jy24—3988

## COMOX ELECTORAL DISTRICT

Discontinuing and Closing Road Allowance for Portion of Beach Access at Hutton Road, Vicinity of Comox

NOTICE is given, pursuant to section 9 of the *Highway Act*, that all that part of road allowance adjacent to Lot 2, District Lot 191, Comox District, Plan 3947, as shown outlined in green on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 6610" (being a copy of Highway District Plan 64-11-79), is hereby discontinued and closed.

ALEX. V. FRASER  
Minister of Transportation  
and Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., July 3, 1980.  
File 64-20-34/340576 jy24—3986

## COWICHAN-MALAHAT ELECTORAL DISTRICT

Establishing Right of Way for Trans-Canada Highway, Vicinity of Bamerton and Mill Bay Road

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all those portions of Blocks 371 and 1237; Block 176 (except Plans 249RW, 13486 and 13743); and Block 176, Plan 249RW; all Malahat District, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 6601" (being a copy of Highway District Plan 5-169), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation  
and Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., June 26, 1980.  
File 62-20-07/340486 jy24—3986

## NANAIMO ELECTORAL DISTRICT

Establishing Right of Way for Waldbank and Applecross Roads, City of Nanaimo

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all those portions of District Lot 48 (except Lot 1, Plan 27659, Lot 1, Plan 27660, and Lot 1, Plan 25547); that part of Lot 53, lying east of Plan 1972 and west of a boundary parallel to the east boundary of said lot extending from a point on the south boundary thereof, 9.625 chains from the southeast corner of said lot (except Plan 18209); all within Wellington District, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 6599" (being a copy of Highway District Plan 61-1-120), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation  
and Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., June 26, 1980.  
File 61-20-00/340195 jy24—3987

c.c. District Forester, Vancouver, Attention: Engineering -  
re Project 7510  
✓ c.c. Ranger G.D. Bertram, Box 938, Hope  
c.c. Regional Property Negotiator, Department of Highways,  
4333 Ledger Street, Burnaby 2 - Attention: Mr. V. Scanlon  
c.c. Mr. E.E. Readshaw, Director of Design and Survey, Depart-  
ment of Highways, Buildings

July 26, 1973

File 0283556 ✓



Mr. D. W. Low,  
Sunshine Valley Developments Ltd.,  
3737 Napier Street,  
Burnaby 2, British Columbia.

RECEIVED  
REGIONAL PROPERTY NEGOTIATOR

JUL 31 1973

Dear Sir:

ANSWERED.....  
NOTED.....  
DATE.....

Re: Sumallo River Forest Road

Thank you for your letter of June 12, 1973. To answer your first query in that letter, we have confirmation that the forest road right-of-way has been registered in the Kamloops Land Registry Office under document No. H-17471.

We can appreciate your desire to expedite the establishment of legal public access acceptable to the Department of Highways and the Land Registry Office, and to this end we have foregone our usual policy of not carrying out legal survey until after completion of road construction and have requested a British Columbia Land Surveyor to survey the portion of forest road right-of-way as shown in red on our enclosed Drawing 1-35.

We understand from the Department of Highways that the junction of the forest road with No. 3 Provincial Highway has recently been discussed with you and agreed upon as shown on Department of Highways' drawings R2-22-9 and R2-22-10. The B.C. Land Surveyor has been instructed to pin this junction area according to said plans and as shown outlined in green on the enclosed copy.

We have no objection to your proposed new junctions outlined in the last paragraph of your above mentioned letter.

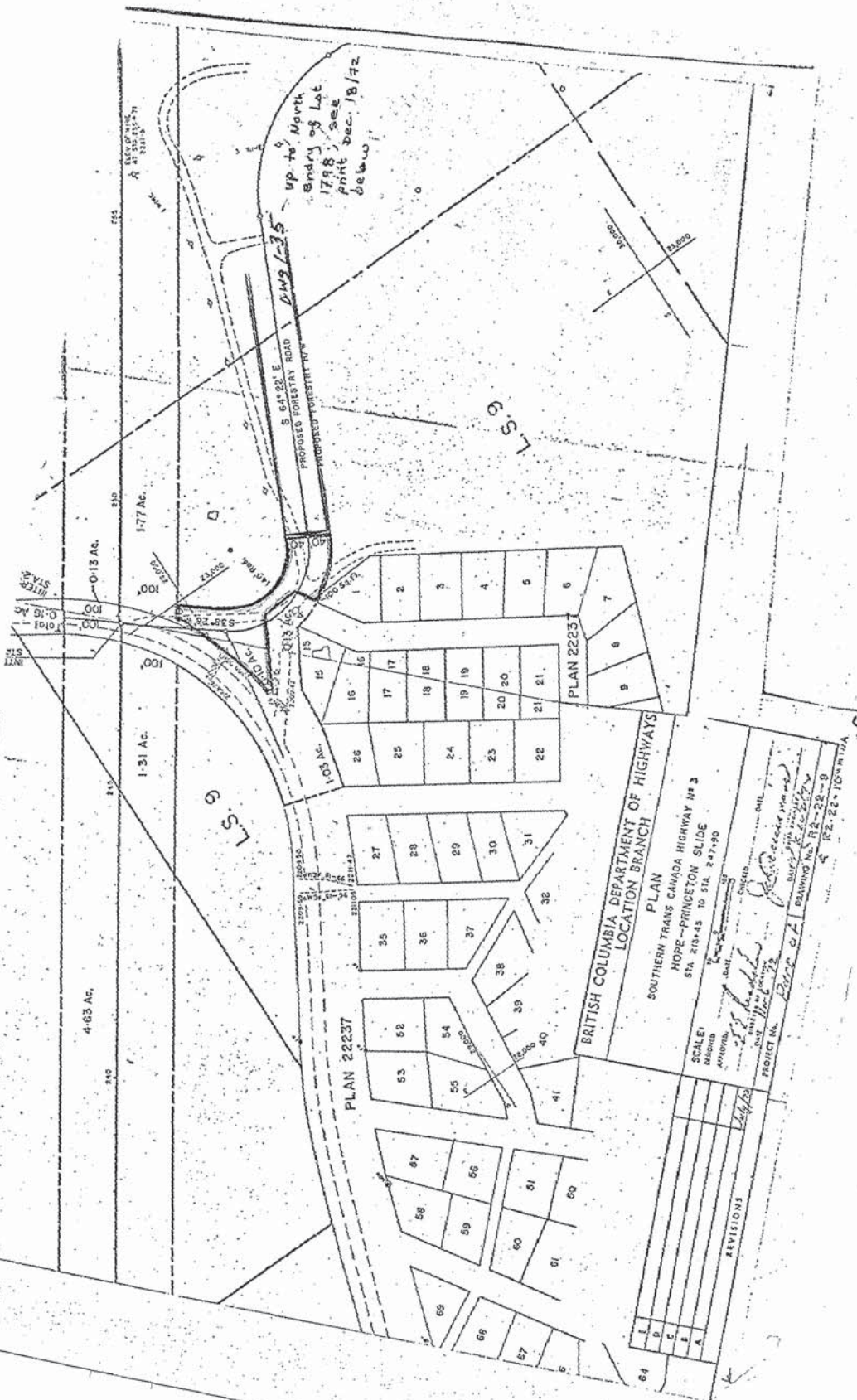
A letter has been sent to Silver Tip Development Ltd. advising of the legal survey expected to take place in August and of the necessity to protect the legal survey pins and monuments.

Yours truly,

R. F. Bryant, P.Eng.,  
for Acting Engineer-in-charge,  
Engineering Division.

RCG:su  
Att.





Fußw. unter 12 Lfd.



Province of  
British Columbia

Ministry of  
Transportation  
and Highways  
HIGHWAYS

7818 - 6th Street  
Burnaby, B.C.  
V3N 4N8

Telephone: 525-0961

YOUR FILE: .....

OUR FILE: P/N 22943

October 21, 1981

Sunshine Valley Developments Ltd.  
283 East 11th Avenue  
Vancouver, B.C.  
V5T 2C4

Attention: Mr. Don W. Low  
President

Dear Sir:

Re: October 1, 1981 Gazette Notice

Further to your letter of October 19, 1981, enclosed please  
find a print of plan H15930 as per your request.

Yours truly,

Lionel H. Mercier  
Regional Property Negotiator

Per:

J.D. Killer  
Property Negotiator

JDK/bc

Encl.

NOTICE is given, pursuant to section 9 of the *Highway Act*, that all that part of Keefe's Landing Road 59 within District Lot 476, Range 4, Coast District, as shown outlined in green on a plan of survey recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7541" (being a copy of plan deposited in the Prince Rupert Land Title Office as Plan 9980), is hereby discontinued and closed.

ALEX. V. FRASER  
Minister of Transportation and  
Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., September 24, 1981.  
File 55-01-78/340293 oc1—6586

#### NELSON-CRESTON ELECTORAL DISTRICT

Establishing Right of Way for Turner  
Road 336 and Verigin Road 356

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all that part of Lot 8, District Lot 6305, Plan 791, Kootenay District, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7548" (being a copy of Highway District Plan 1-356), is hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation and  
Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., September 24, 1981.  
File 31-20-00/340267 oc1—6586

#### OKANAGAN SOUTH ELECTORAL DISTRICT

Establishing Right of Way for  
Belgo Road

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all those portions of Lots 3, 4 and 5, Block 17, Section 11; Lot 6; Lot 7 (except Plan 31521); both Block 17, Section 14; Lot 6, Block 16, Section 11 (except Plan B1528); all within Township 26, ODYD, Plan 1380, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7534" (being a copy of Highway District Plan 29-323 R/W), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation and  
Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., September 17, 1981.  
File 24-20-24/340084 oc1—6586

#### YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right of Way for Hope-  
Princeton Highway, Vicinity of Hope  
Slide Viewpoint

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all that part of the Hope-Princeton Highway, lying within Legal Subdivision 2, Section 14, Township 4, Range 25, W6M, YDYD, shown as Parcel A, as shown outlined in bold black line on a plan of survey recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7550" (being a copy of reference plan deposited in the Kamloops Land Title Office as Plan H15930), is hereby established as public highway.

Discontinuing and Closing Road Allowance  
for Portion of Hope-Princeton High-  
way, Vicinity of Hope Slide Viewpoint

NOTICE is given, pursuant to section 9 of the *Highway Act*, that all that part of the Hope-Princeton Highway, lying within Legal Subdivision 2, Section 14, Township 4, Range 25, W6M, YDYD, shown as Parcel A, as shown outlined in bold black line on a plan of survey recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7550" (being a copy of reference plan deposited in the Kamloops Land Title Office as Plan H15930), is hereby discontinued and closed.

ALEX. V. FRASER  
Minister of Transportation and  
Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., October 1, 1981.  
File 15-01-78/340480 oc1—6583

#### PRINCE RUPERT ELECTORAL DISTRICT

Establishing Right of Way for  
Queen Charlotte-Masset Road

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all those portions of Lots 19 to 21 (incl.) of Block 8; Lots 1 to 18 (incl.) of Block 5; Lot 1 of Block 7; Lots 1 to 18 (incl.) of Block 4; Lots 13 to 17 and 19 to 25 (incl.) of Block 3; Lots 8 to 11 and Lots 16 to 18 (incl.) of Block 2; Lots 1 to 24 (incl.) of Block 1; all within Plan 948, District Lot 3; District Lot 2968; District Lot 2972; unsurveyed foreshore contiguous to District Lot 3; Lot 12 of Block 2; undivided part of Lot 18; Lots 1 and 2, Plan 5464; unsurveyed foreshore contiguous to District Lot 18; Block J, Plan 1635; Lot 3, Plan 1793; Block C, Plan 1483; Block F, Plan 1571; Lot 1 of Block F, Plan 1796; Lot 1, Plan 4206; old road within Lot 15A; Lot 2, Plan 4206; Block B, Plan 1415; Block D, Plan 1571; Block A, Plan 1415; Block E, Plan 1571; Block G, Plan 1571 of Lot 15 and Lot 15A; all within District Lot 15A; unsurveyed foreshore contiguous to District

Lot 15A; remainder of Block C, Plan 1635; Lots 1 and 2, Block C, Plan 4537; Block H, Plan 1571; Lots 1 and 2, Plan 1684; Lot 1 of Block B, Plan 3096; Parcels 1 to 3 (incl.) of Lot 2, Block B, Plan 3514; Lot A of Parcel 4 of Lot 2, Block B, Plan 5062; remainder of Parcel 4 of Lot 2, Block B, Plan 5062; Parcels 5 and 6 of Lot 2, Block B, Plan 3514; Lot A of Block B, Plan 5107; remainder of Lot 3 of Block B, Plan 3096; Lots 4 and 5 of Block B, Plan 3096; Lot 3 of Block 10, Plan 3108; remainder of Lot 4, Block 10, Plan 3108; westerly portion of Lot 4, Block 10, Plan 15189; District Lot 15; Lot 2 of Block 10, Plan 3108; Parcels 1 and 3 of Lot 1, Block 10, Plan 3454; Block 3, Plan 3735; Lots 1 and 2 of Block 3, Plan 3735; easterly 300 feet of Block 9, Plan 1414; Block 9, except easterly 300 feet, Plan 1414; Lot 15 of Block 5, Plan 934A; Block 3, Plan 1414; Lot 2872; unsurveyed foreshore contiguous to District Lot 15; all within District Lot 15; Block A, Plan 1661; Lot 309; Block 10, Plan 934; parcel of Block 10, Plan 3031; Lots 9 to 12 (incl.) of Block 26, Plan 934; Lots 13 to 22 (incl.) of Block 12, Plan 934; Lot 1, Block 24, Plan 934; Lots 1 to 3 of Block 23, Plan 934; Lots 6 to 15 (incl.) of Block 22, Plan 934; all within District Lot 16A; unforeshore contiguous to District Lot 16A; District Lot 16A; all within Queen Charlotte District, as shown outlined in red on a plan of survey recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7538" (being a copy of plan deposited in the Prince Rupert Land Title Office as Plan 6937), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation and  
Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., September 17, 1981.  
File 51-20-08/340081 oc1—6585

#### SHUSWAP-REVELSTOKE ELECTORAL DISTRICT

Establishing Right of Way for  
Cobex Road 102

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all that part of Legal Subdivision 12, Section 20, Township 22, Range 11, W6M, KDYD, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7546" (being a copy of Highway District Plan 1200), is hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation and  
Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., September 24, 1981.  
File 22-20-31/340265 oc1—6585





Province of  
British Columbia

Ministry of  
Transportation  
and Highways  
HIGHWAYS

7818 - 6th Street  
Burnaby, B.C.  
V3N 4N8

Telephone: 525-0961

YOUR FILE: .....

OUR FILE: P/N 22943

October 21, 1981

Sunshine Valley Developments Ltd.  
283 East 11th Avenue  
Vancouver, B.C.  
V5T 2C4

Attention: Mr. Don W. Low  
President

Dear Sir:

Re: October 1, 1981 Gazette Notice

Further to your letter of October 19, 1981, enclosed please  
find a print of plan H15930 as per your request.

Yours truly,

Lionel H. Mercier  
Regional Property Negotiator

Per:

J.D. Killer  
Property Negotiator

JDK/bc

Encl.

NOTICE is given, pursuant to section 9 of the *Highway Act*, that all that part of Keefe's Landing Road 59 within District Lot 476, Range 4, Coast District, as shown outlined in green on a plan of survey recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7541" (being a copy of plan deposited in the Prince Rupert Land Title Office as Plan 9980), is hereby discontinued and closed.

ALEX. V. FRASER  
Minister of Transportation and  
Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., September 24, 1981.  
File 55-01-78/340293 oc1—6586

### NELSON-CRESTON ELECTORAL DISTRICT

Establishing Right of Way for Turner  
Road 336 and Verigin Road 356

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all that part of Lot 8, District Lot 6305, Plan 791, Kootenay District, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7548" (being a copy of Highway District Plan 1-356), is hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation and  
Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., September 24, 1981.  
File 31-20-00/340267 oc1—6586

### OKANAGAN SOUTH ELECTORAL DISTRICT

Establishing Right of Way for  
Belgo Road

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all those portions of Lots 3, 4 and 5, Block 17, Section 11; Lot 6; Lot 7 (except Plan 31521); both Block 17, Section 14; Lot 6, Block 16, Section 11 (except Plan B1528); all within Township 26, ODYD, Plan 1380, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7534" (being a copy of Highway District Plan 29-323 R/W), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation and  
Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., September 17, 1981.  
File 24-20-24/340084 oc1—6586

### YALE-LILLOOET ELECTORAL DISTRICT

Establishing Right of Way for Hope-  
Princeton Highway, Vicinity of Hope  
Slide Viewpoint

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all that part of the Hope-Princeton Highway, lying within Legal Subdivision 2, Section 14, Township 4, Range 25, W6M, YDYD, shown as Parcel A, as shown outlined in bold black line on a plan of survey recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7550" (being a copy of reference plan deposited in the Kamloops Land Title Office as Plan H15930), is hereby established as public highway.

Discontinuing and Closing Road Allowance  
for Portion of Hope-Princeton High-  
way, Vicinity of Hope Slide Viewpoint

NOTICE is given, pursuant to section 9 of the *Highway Act*, that all that part of the Hope-Princeton Highway, lying within Legal Subdivision 2, Section 14, Township 4, Range 25, W6M, YDYD, shown as Parcel A, as shown outlined in bold black line on a plan of survey recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7550" (being a copy of reference plan deposited in the Kamloops Land Title Office as Plan H15930), is hereby discontinued and closed.

ALEX. V. FRASER  
Minister of Transportation and  
Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., October 1, 1981.  
File 15-01-78/340480 oc1—6583

### PRINCE RUPERT ELECTORAL DISTRICT

Establishing Right of Way for  
Queen Charlotte-Masset Road

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all those portions of Lots 19 to 21 (incl.) of Block 8; Lots 1 to 18 (incl.) of Block 5; Lot 1 of Block 7; Lots 1 to 18 (incl.) of Block 4; Lots 13 to 17 and 19 to 25 (incl.) of Block 3; Lots 8 to 11 and Lots 16 to 18 (incl.) of Block 2; Lots 1 to 24 (incl.) of Block 1; all within Plan 948, District Lot 3; District Lot 2968; District Lot 2972; unsurveyed foreshore contiguous to District Lot 3; Lot 12 of Block 2; undivided part of Lot 18; Lots 1 and 2, Plan 5464; unsurveyed foreshore contiguous to District Lot 18; Block J, Plan 1635; Lot 3, Plan 1793; Block C, Plan 1483; Block F, Plan 1571; Lot 1 of Block F, Plan 1796; Lot 1, Plan 4206; old road within Lot 15A; Lot 2, Plan 4206; Block B, Plan 1415; Block D, Plan 1571; Block A, Plan 1415; Block E, Plan 1571; Block G, Plan 1571 of Lot 15 and Lot 15A; all within District Lot 15A; unsurveyed foreshore contiguous to District

Lot 15A; remainder of Block C, Plan 1635; Lots 1 and 2, Block C, Plan 4537; Block H, Plan 1571; Lots 1 and 2, Plan 1684; Lot 1 of Block B, Plan 3096; Parcels 1 to 3 (incl.) of Lot 2, Block B, Plan 3514; Lot A of Parcel 4 of Lot 2, Block B, Plan 5062; remainder of Parcel 4 of Lot 2, Block B, Plan 5062; Parcels 5 and 6 of Lot 2, Block B, Plan 3514; Lot A of Block B, Plan 5107; remainder of Lot 3 of Block B, Plan 3096; Lots 4 and 5 of Block B, Plan 3096; Lot 3 of Block 10, Plan 3108; remainder of Lot 4, Block 10, Plan 3108; westerly portion of Lot 4, Block 10, Plan 15189; District Lot 15; Lot 2 of Block 10, Plan 3108; Parcels 1 and 3 of Lot 1, Block 10, Plan 3454; Block 3, Plan 3735; Lots 1 and 2 of Block 3, Plan 3735; easterly 300 feet of Block 9, Plan 1414; Block 9, except easterly 300 feet, Plan 1414; Lot 15 of Block 5, Plan 934A; Block 3, Plan 1414; Lot 2872; unsurveyed foreshore contiguous to District Lot 15; all within District Lot 15; Block A, Plan 1661; Lot 309; Block 10, Plan 934; parcel of Block 10, Plan 3031; Lots 9 to 12 (incl.) of Block 26, Plan 934; Lots 13 to 22 (incl.) of Block 12, Plan 934; Lot 1, Block 24, Plan 934; Lots 1 to 3 of Block 23, Plan 934; Lots 6 to 15 (incl.) of Block 22, Plan 934; all within District Lot 16A; unforeshore contiguous to District Lot 16A; District Lot 16A; all within Queen Charlotte District, as shown outlined in red on a plan of survey recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7538" (being a copy of plan deposited in the Prince Rupert Land Title Office as Plan 6937), are hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation and  
Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., September 17, 1981.  
File 51-20-08/340081 oc1—6585

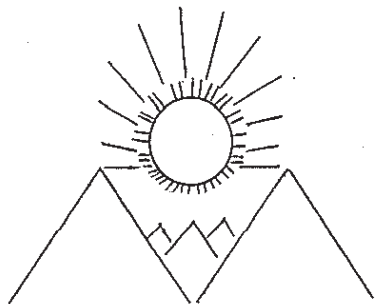
### SHUSWAP-REVELSTOKE ELECTORAL DISTRICT

Establishing Right of Way for  
Cobex Road 102

NOTICE is given, pursuant to section 6 of the *Highway Act*, that all that part of Legal Subdivision 12, Section 20, Township 22, Range 11, W6M, KDYD, as shown outlined in red on a plan recorded in the Provincial Ministry of Transportation and Highways, Victoria, B.C., under "Road Surveys 7546" (being a copy of Highway District Plan 1200), is hereby established as public highway.

ALEX. V. FRASER  
Minister of Transportation and  
Highways

Ministry of Transportation  
and Highways,  
Parliament Buildings,  
Victoria, B.C., September 24, 1981.  
File 22-20-31/340265 oc1—6585



# Sunshine Valley Developments Ltd.

12743

November 2, 1973.

Department of Highways,  
4333 Ledger Street,  
Burnaby 2, B.C.

RECEIVED  
REGIONAL PROPERTY NEGOTIATOR

NOV - 6 1973

RECEIVED  
DEPT. OF HIGHWAYS  
BURNABY, B. C.

NOV 6 A.M.

Attention: Mr. H. R. MacKenzie  
Vancouver Regional Property Negotiator

ANSWERED.....

NOTED.....

ANSWERED.....

NOTED.....

DATE.....

Re: Highway Relocation-Sunshine Valley  
Your File: 292/258826

Dear Sirs:

In follow-up of our letter of January 4, 1973, and various meetings with Mr. V. Scanlon, we are now interested in obtaining the following information, etc. from you:

1. Confirmation of the route of the re-location highway.
2. Confirmation as to location and placement of the second culvert underpass, for foot traffic.
3. A confirmation as to the cost of the waterway underpass culvert as previously agreed.
4. Confirmation that your right-of-way will, of necessity, make it necessary for you to acquire Lot 93 in our Cedar Village subdivision, since the tentative arrangement with our purchaser of this lot to re-locate, expires at November 30, 1973.

It is essential that we have an early reply to this letter with the above mentioned detail, since our plans adjacent to the highway, have been held in 'limbo' during the whole of this year.

Yours very truly,

SUNSHINE VALLEY DEVELOPMENTS LTD.

Per: D.W. Low

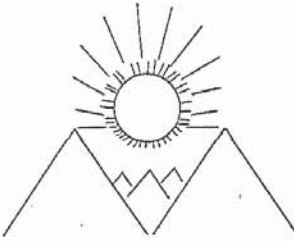
DWL: aey

283 EAST 11TH AVENUE, VANCOUVER 10, BRITISH COLUMBIA

(604) 873-3377







# Sunshine Valley Developments Ltd.

22743

November 2, 1973.

Department of Highways,  
4333 Ledger Street,  
Burnaby 2, B.C.

RECEIVED  
REGIONAL PROPERTY NEGOTIATOR

NOV - 6 1973

ANSWERED.....

NOV 6 A.M.

Attention: Mr. H.R. MacKenzie  
Vancouver Regional Property Negotiator

ANSWERED.....

NOTED.....

DATE.....

Re: Highway Relocation-Sunshine Valley  
Your File: 292/258826

Dear Sirs:

In follow-up of our letter of January 4, 1973, and various meetings with Mr. V. Scanlon, we are now interested in obtaining the following information, etc. from you:

1. Confirmation of the route of the re-location highway. ?
2. Confirmation as to location and placement of the second culvert underpass, for foot traffic. \$9,500
3. A confirmation as to the cost of the waterway underpass culvert as previously agreed. \$6300 ?
4. Confirmation that your right-of-way will of necessity, make it necessary for you to acquire Lot 93 in our Cedar Village subdivision, since the tentative arrangement with our purchaser of this lot to re-locate, expires at November 30, 1973. not require entire lot

It is essential that we have an early reply to this letter with the above mentioned detail, since our plans adjacent to the highway, have been held in 'limbo' during the whole of this year.

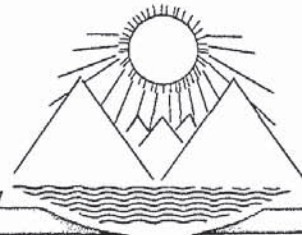
Yours very truly,

SUNSHINE VALLEY DEVELOPMENTS LTD.

Per: D.W. Low

DWL:aey

283 EAST 11TH AVENUE, VANCOUVER 10, BRITISH COLUMBIA (604) 873-3377



Administration Division  
Ministry of Highways & Public Works  
940 Blanshard Street  
Victoria, B. C.

November 9th, 1978

Plan #106-94

P/N 22943

Attn: K. Muir, Property Services Division

Re: Establishing Right-of-Way for Southern  
Trans Provincial Highway Hope-Princeton  
Section

&

Notice of Amendment

It is requested that the above-mentioned highway right-of-way be expropriated pursuant to the Highway Act and according to Ministry regulations.

1. Attached draft notice in duplicate for expropriation purposes.
2. Settlement has been reached with the property owner.
3. Notice required to "legalize" right-of-way.
4. Recommended by the Regional Highway Engineer.
5. Sepia and two-paper prints of Legal Surveys Plan H-13829 are enclosed showing areas to be gazetted outlined in red.

Notice of Amendment also enclosed cancelling prior notice of February 2nd, 1978.

L. H. Mercier  
Regional Property Negotiator

Recommended by:

P. J. Carr  
Regional Highway Engineer

JDK/uan

encls.

YALE LILLOOET ELECTORAL DISTRICT

Establishing additional Right-of-Way for Southern  
Trans Provincial Highway Hope-Princeton Section

(Notice of Amendment)

NOTICE is given, pursuant to Section 8 (2) of the  
"Highway Act", and all other powers thereunto enabling, that  
notice dated January 19th, 1978, and published in the B. C.  
Gazette of February 2nd, 1978, and recorded in the Provincial  
Ministry of Highways & Public Works, Victoria, B. C., under  
"Road Surveys 3329-1 is hereby cancelled.



Chief Property Negotiator,  
Department of Highways,  
Victoria.

Regional Office, Burnaby  
V5G 3T3  
November 13th, 1973.  
Yale Lillooet

P/N 22943

Hope Princeton Slide Property  
Sunshine Valley Developments Ltd.

Attached hereto please find a copy of a letter dated  
November 2nd, 1973, as received from Sunshine Valley Developments Ltd.

As the file has now been turned over to the Departmental  
Solicitor, some of the points raised in the Company's letter may have  
to be referred before confirming.

The attached copy of the letter of August 17th, 1973  
from the Director of Highway Design and Surveys confirms the costs  
and location of culverts on Sheets R2-22-10 and R2-22-11. However,  
there is a proposed culvert at station 231+50 (Sheet R2-22-9), which  
the Company had requested be enlarged for an underpass and this will  
require confirmation. I believe there was additional cost to the  
claimant on this culvert in the amount of \$6,300. The right-of-way  
in Log 93 (Sheet R2-22-9) has been reduced sufficiently so that it  
would not be necessary to purchase the lot.

May we please be advised if there are any revisions  
contemplated on Sheets R2-22-5 to R2-22-8 and R2-22-12 before  
confirming the points raised in the claimant's attached letter.

H. R. MacKenzie,  
Regional Property Negotiator.

VS/dp  
Encl.

DEPARTMENT OF HIGHWAYS AND PUBLIC WORKS

**CLOSING ROAD ALLOWANCE BY GAZETTE NOTICE  
(OTHER THAN BY PLANS CANCELLATION)**

Date November 22, 1979  
Electoral Dist. Yale-Lillooet  
Highway Dist. Hope  
Headquarters File \_\_\_\_\_  
Regional File PN 22943  
District File 16-21-62  
Reference \_\_\_\_\_  
Dated \_\_\_\_\_

- |   | YES                                 | NO                                  |
|---|-------------------------------------|-------------------------------------|
| 1. Description and plan attached. _____   | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| 2. Final disposition will be carried out under section 11, subsection (2), of the <i>Highway Act</i> . _____  | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| 3. Is closure required in order to meet the requirements of section 113 of the <i>Land Registry Act</i> ? _____   | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| 4. Has consideration been given to the sale of the closed portion of road allowance? _____  | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| 5. Is an Order in Council required by the Land Registry Office authorizing the transfer of closed portion of road allowance to the adjacent property-owner? _____ | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| 6. A check has been made to see that there are no public utilities or private works on the right-of-way involved. _____   | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| 7. Have regular users and adjacent property-owners been informed of intent to close? _____  | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| 8. Objections, if any, attached. _____  | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| 9. A check has been made to ensure that this closure does not cut off access to any individual parcel of land. _____  | <input type="checkbox"/>            | <input type="checkbox"/>            |
| 10. If closure is within a municipal area, has the municipality signified approval? _____   | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| 11. Statement as to reasons for closure and comment on objections if any:   |                                     |                                     |

*Property is being exchanged for new right of way for Southern Trans Provincial Highway #3. It appears that individual parcels (Legal Subdivision) may be cut off. The District has no requirements for the property.*

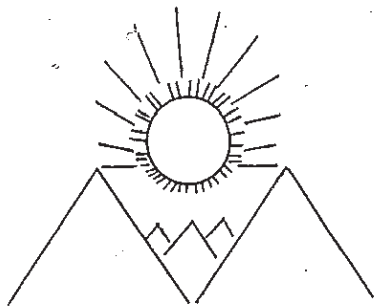
Recommended:

*P.S. Dunn*

District Official.

Regional Engineer.

Right-of-way Agent.



Sunshine Valley

Developments Ltd.

RECEIVED  
REGIONAL PROPERTY NEGOTIATOR

December 23, 1981.

JAN 4 1982

Ministry of Transportation and Highways,  
7818 - 6th Street,  
Burnaby, B.C.  
V3N 4N8

ANSWERED .....

NOTED .....

DATE .....

Attention: Mr. Lionel H. Mercier - Regional Property Negotiator

Dear Sir:

re: October 1, 1981 Gazette Notice (copy enclosed)

We have Jim Killer's letter of October 21, 1981 (copy enclosed) enclosing the print of plan H15930. He called us later on to say that this plan had been submitted to someone in Victoria to raise the Title.

This is now two months later, and we have not heard a word, and we would appreciate knowing when and if this Title is going to be issued in the name of Sunshine Valley Developments Ltd. We would like to have a confirmation from whomever your Department deals with, and the Land Office that in fact, the Title has been placed in our name.

The Regional District is still holding up our zoning until we have Title, and we must have this completed at an early date.

Yours truly,

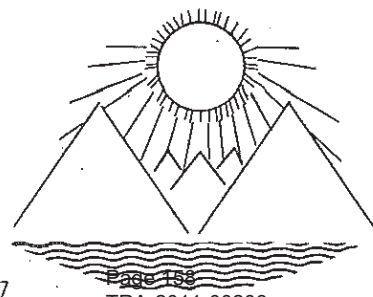
SUNSHINE VALLEY DEVELOPMENTS LTD.,

*Don W. Low*

Per: Don W. Low,  
President

DWL/b

Encl.





Transportation, Communication and Highways  
Property Negotiations  
3876 Norland Avenue  
Burnaby, B.C., V5G 3S8  
Telephone : 294-4711

P/N 22943  
Yale-Lillooet

Sunshine Valley Developments Ltd.,  
283 East 11th Avenue  
Vancouver, B.C.  
V5T 2C4

Attention : Don W. Low - President

Dear Sir,

Re: Hope Slide Viewpoint

Your letter to Mr. R.G. Harvey, Deputy Minister relative to the above and dated February 8th 1979, has been forwarded to this office for reply.

Complying with your requests therein, please find enclosed copy of legal survey plan H.13829 prepared by Messrs Tunbridge, which is a revised plan of this Ministry's requirements, at the viewpoint and was deposited at the Kamloops Land Registry Office on October 16th 1978. Said plan indicates present access and area of requirement.

Also included is a copy of the notice which was published December 7th 1978, in the B.C. Gazette and which cancels and supersedes all previous notices.

We trust that the foregoing information is satisfactory and sufficient for you to proceed with your proposed building plans.

Should you require further or additional information, please contact the undersigned. Thank you.

Yours truly  
Lionel H. Mercier  
Regional Property Negotiator

By:

J.D. Killer  
Property Negotiator

JHK/kbv  
encl.

c.c. Director of Property Services,  
Victoria, B.C.

YALE LILLOOET ELECTORAL DISTRICT

Establishing additional Right-of-way for Southern  
Trans Provincial H'ghway Hope to Princeton Section

NOTICE is given, pursuant to Section 8 of the  
"Highway Act", that all that portion of Legal Subdivision  
7 of Section 14, Township 4, Range 25, West 6th Meridian,  
Yale Division Yale District, as shown outlined in red  
on a plan recorded in the Provincial Ministry of Highways &  
Public Works, Victoria, B. C., under "Road Surveys ....."  
(being a copy of a plan deposited in the Kamloops Land  
Registry as Plan H-13829) is hereby established as public  
highway.

N H-145

## BOOK OF REFERENCE FOR CROWN AND PRIVATE LANDS.

AREA REQ'D.	OWNER	CROWN / PRIVATE	LOT	L.S.	1/4	SEC.	TR	RGE.	MER.	DIST.	PLAN	AREA REQ'D.
5 1.331 ha.	CROWN				N.W.	14	4	25	W6.M.	Y.D.Y.D.	7679L	0.4394 ha.
0.5667 ha.	PRIVATE			10		14	4	25	W6.M.	Y.D.Y.D.	2.371 ha.	2.377 ha.
2.712 ha.	CROWN				S.W.	14	4	25	W6.M.	Y.D.Y.D.	0.0707 m <sup>2</sup>	0.0707 m <sup>2</sup>
1.995 ha.	PRIVATE			7		14	4	25	W6.M.	Y.D.Y.D.	3.031 ha.	3.031 ha.
0.5059 ha.	CROWN			2		14	4	25	W6.M.	Y.D.Y.D.	2.758 ha.	2.758 ha.
1.485 ha.	PRIVATE			1		14	4	25	W6.M.	Y.D.Y.D.	0.7821	0.7821 ha.
3.480 ha.	CROWN			16		11	4	25	W6.M.	Y.D.Y.D.	3.010 ha.	3.010 ha.
0.1619 ha.	CROWN			9		11	4	25	W6.M.	Y.D.Y.D.	1.116 ha.	1.116 ha.
1.453 ha.	PRIVATE	OLD R/W NOT DECEASED	(0.0087 ha)	12		12	4	25	W6.M.	Y.D.Y.D.	2.447 ha.	2.447 ha.
0.7325 ha.	CROWN	11	(0.6070)	5		12	4	25	W6.M.	Y.D.Y.D.	1.936 ha.	1.936 ha.
0.9591 ha.	PRIVATE		(0.3885)	6		12	4	25	W6.M.	Y.D.Y.D.	1.170 ha.	1.170 ha.
462.9 m <sup>2</sup>	PRIVATE			3		12	4	25	W6.M.	Y.D.Y.D.	2.761 ha.	2.761 ha.
	PRIVATE			2		12	4	25	W6.M.	Y.D.Y.D.	0.5883 ha.	0.5883 ha.
	PRIVATE			15		1	4	25	W6.M.	Y.D.Y.D.	2.435 ha.	2.435 ha.
	PRIVATE			16		1	4	25	W6.M.	Y.D.Y.D.	1.892 ha.	1.892 ha.
	PRIVATE			9		1	4	25	W6.M.	Y.D.Y.D.	1.727 ha.	1.727 ha.
	PRIVATE			12		6	4	24	W6.M.	Y.D.Y.D.	2.997 ha.	2.997 ha.
	PRIVATE			11		6	4	24	W6.M.	Y.D.Y.D.	0.2356 ha.	0.2356 ha.
	PRIVATE			5		6	4	24	W6.M.	Y.D.Y.D.	0.02123 ha.	0.02123 ha.
	PRIVATE			6		6	4	24	W6.M.	Y.D.Y.D.	2.694 ha.	2.694 ha.
	PRIVATE			7		6	4	24	W6.M.	Y.D.Y.D.	2.527 ha.	2.527 ha.
	PRIVATE			2		6	4	24	W6.M.	Y.D.Y.D.	1.564 ha.	1.564 ha.
	PRIVATE			1		6	4	24	W6.M.	Y.D.Y.D.	2.765 ha.	2.765 ha.
	CROWN			1		6	4	24	W6.M.	Y.D.Y.D.	PLAN 9482	0.2430 ha.
	CROWN			4		5	4	24	W6.M.	Y.D.Y.D.	0.1863 ha.	0.1863 ha.
	PRIVATE			16		31	3	24	W6.M.	Y.D.Y.D.	0.03304 ha.	0.03304 ha.
	PRIVATE			13		32	3	24	W6.M.	Y.D.Y.D.	3.181 ha.	3.181 ha.
	CROWN (UNSURVEYED) EAST OF L.S. 13					32	3	24	W6.M.	Y.D.Y.D.	5.760 ha.	5.760 ha.

BOOK OF REFERENCE FOR ADDITIONAL LANDS ACQUIRED



3.480 ha.

CROWN	16.	11.	4.	25.	W6.M.	Y.D.Y.D.	3.010 ha	3.010 ha	2.940 ha
CROWN	9.	11.	4.	25.	W6.M.	Y.D.Y.D.	1.116 ha	1.116 ha	
PRIVATE	12.	12.	4.	25.	W6.M.	Y.D.Y.D.	2.447 ha	2.447 ha	
CROWN	5.	12.	4.	25.	W6.M.	Y.D.Y.D.	1.936 ha	1.936 ha	
PRIVATE	6.	12.	4.	25.	W6.M.	Y.D.Y.D.	1.170 ha	1.170 ha	
PRIVATE	3.	12.	4.	25.	W6.M.	Y.D.Y.D.	2.761 ha	2.761 ha	
PRIVATE	2.	12.	4.	25.	W6.M.	Y.D.Y.D.	0.5883 ha	0.5883 ha	
PRIVATE	15.	1.	4.	25.	W6.M.	Y.D.Y.D.	2.435 ha	2.435 ha	
PRIVATE	16.	1.	4.	25.	W6.M.	Y.D.Y.D.	1.892 ha	1.892 ha	
PRIVATE	9.	1.	4.	25.	W6.M.	Y.D.Y.D.	1.727 ha	1.727 ha	
PRIVATE	12.	6.	4.	24.	W6.M.	Y.D.Y.D.	2.997 ha	2.997 ha	
PRIVATE	11.	6.	4.	24.	W6.M.	Y.D.Y.D.	0.2356 ha	0.2356 ha	
PRIVATE	5.	6.	4.	24.	W6.M.	Y.D.Y.D.	0.02123 ha	0.02123 ha	
PRIVATE	6.	6.	4.	24.	W6.M.	Y.D.Y.D.	2.694 ha	2.694 ha	
PRIVATE	7.	6.	4.	24.	W6.M.	Y.D.Y.D.	2.527 ha	2.527 ha	
PRIVATE	2.	6.	4.	24.	W6.M.	Y.D.Y.D.	1.564 ha	1.564 ha	
PRIVATE	1.	6.	4.	24.	W6.M.	Y.D.Y.D.	2.765 ha	2.765 ha	
CROWN	1.	6.	4.	24.	W6.M.	Y.D.Y.D.	0.2430 ha	0.2430 ha	
CROWN	4.	5.	4.	24.	W6.M.	Y.D.Y.D.	0.1863 ha	0.1863 ha	
PRIVATE	16.	31.	3.	24.	W6.M.	Y.D.Y.D.	0.03304 ha	0.03304 ha	
PRIVATE	13.	32.	3.	24.	W6.M.	Y.D.Y.D.	3.181 ha	3.181 ha	
CROWN (UNSURVEYED) EAST OF L.S.13		32.	3.	24.	W6.M.	Y.D.Y.D.	5.760 ha	5.760 ha	

# BOOK OF REFERENCE FOR ADDITIONAL LANDS ACQUIRED

CROWN	13/16	6/1	4/4	24/25	W6.M./W6.M.	Y.D.Y.D./Y.D.Y.D.	0.813 ha	0.813 ha
PRIVATE	7	14.	4.	25.	W6.M.	Y.D.Y.D.	2.161 ha	2.161 ha
PRIVATE	1	14.	4.	25.	W6.M.	Y.D.Y.D.	9.43.1 m <sup>2</sup>	9.43.1 m <sup>2</sup>
PRIVATE	12	12.	4.	25.	W6.M.	Y.D.Y.D.	2.657 ha	2.657 ha
PRIVATE	6	12.	4.	25.	W6.M.	Y.D.Y.D.	0.2431 ha	0.2431 ha
CROWN	16	11.	4	25	W6.M.	Y.D.Y.D.	7.131 ha	7.131 ha

Y: 1983

BCL'S





Conditions for temporary right of way across Sunshine Valley Development Ltd. Lands (approximately Station 285 to Station 310) for detouring traffic while new #3 Highway is under construction.

1. Temporary Highway to be constructed by Commonwealth in location shown on attached drawing. This drawing to be signed by Sunshine Valley Developments Ltd., Commonwealth and Ministry of Highways.
2. Width of Highway right of way to be no greater than 50 feet, except where additional width is required to sufficiently contain the road surface, side, slopes, ditches and backslopes. Road to be built to subdivision standards, acceptable by the Ministry of Highways.
3. Right of way to be cleared, debris burned and all saleable logs properly de-limbed and hauled to Sunshine Valley Developments yard.
4. Debris and rocks are not to be pushed to the side of the right of way nor left in a location that will interfere with subdivision lot development.
5. Culverts to be installed where required; especially at Stations 296 & 10 and 303.
6. Commonwealth to install 12" conduits across temporary Highway and new Highway in location and depth designated by Sunshine Valley Developments Ltd. (probably about Station 286 to 287). Conduit pipe to be supplied by Sunshine Valley Developments Ltd.
7. Surface of temporary Highway (preferably asphalt) to be left in good vehicle travelling condition upon termination of its use.
8. Temporary Highway will be degazetted and returned to Sunshine Valley Developments Ltd. free of charge and including any and all improvements situated thereon, no later than December 31, 1981. All traffic will be removed therefrom not later than December 31, 1981.
9. The remains of the present Highway (approximately between Station 286 and 291) to be levelled out by Commonwealth at Sunshine Valley Developments option. The blacktop on this portion may be salvaged by Commonwealth and/or the Ministry of Highways if desired.



10. Ministry of Highways to assure Sunshine Valley Developments that the waters discharged from the culvert at Station 288 will not flood Sunshine Valley Developments property on the south side of the new Highway. Adequate channels to be constructed to direct the water to existing streams on the south side of the new Highway at no cost to Sunshine Valley Developments Ltd.
11. Temporary Highway to be posted with adequate "No Hunting, No Fishing, No Trespassing" signs and "No Stopping" signs on both sides of the Highway.
12. Ministry of Highways to approve application dated June 18, 1980 for pipeline conduit crossings of new #3 Highway.
13. Ministry of Highways to resolve Viewpoint lease problem per Sunshine Valley Developments letter to Mike O'Connor dated June 30, 1980.
14. Ministry of Highways to resolve Cedar Village frontage road cleanup problem per Sunshine Valley Developments letter to Mike O'Connor dated June 30, 1980.

Dated this \_\_\_\_\_ Day of \_\_\_\_\_ 1980.

Sunshine Valley Developments Ltd.

Commonwealth Construction Co. Ltd.

Ministry of Transportation and Highways

Closure of Cedar Boulevard  
unregistered Plan  
of Plan #17907

ESTABLISHED BY NOTICE
IN B.C. GAZ. 84/10/11
D.F. No. _____
DATE _____
R/W PLAN _____

Cowsey Hwy.

ESTABLISHED BY NOTICE
IN B.C. GAZETTE 84/10/11
D.F. No. _____
DATE _____
R/W PLAN Hwy P. #18302 (16-94A)

Closure of Portions of H-145 & Portions  
shown cross-hatched  
on unregistered survey  
Plan (dated from Plan  
#18302)

ESTABLISHED BY NOTICE
IN B.C. GAZETTE 84/10/11
D.F. No. _____
DATE _____
R/W PLAN _____

ESTABLISHED BY NOTICE
IN B.C. GAZETTE _____
D.F. No. _____
DATE 78/12/8
R/W PLAN #168-94A

DETOUR ROAD

ESTABLISHED BY NOTICE
IN B.C. GAZETTE 80/7/24
D.F. No. _____
DATE 80/6/17
R/W PLAN #16-94(L)

Amendment of Road Survey 8384

ESTABLISHED BY NOTICE
IN B.C. GAZETTE 84/11/29
D.F. No. _____
DATE _____
R/W PLAN _____

ESTABLISHED BY NOTICE
IN B.C. GAZETTE 84/11/29
D.F. No. _____
DATE _____
R/W PLAN _____

Amendment of Road  
Survey 8383

FOR DETAILED PLAN INFORMATION  
ON LAND EXCHANGE SEE  
#16-94A

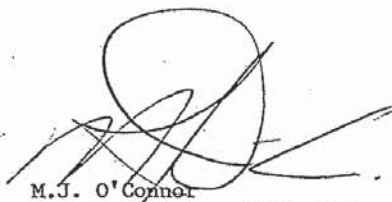
378

LETTER OF UNDERSTANDING

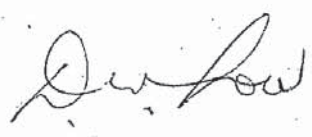
BETWEEN

SUNSHINE VALLEY DEVELOPMENTS LTD. & THE MINISTRY OF TRANSPORTATION & HIGHWAYS

- 1) Upon receipt of a site plan for the commercial building at the Hope Slide with an explanation of the need for adjacent Crown land, the Ministry will prepare a de-gazetting of the necessary Highway Right-of-Way. The land would be sold to Sunshine Valley Developments Ltd. at market value.
- 2) The Ministry will gazette the entire drain field area and the Ministry agree to lease, or allow by easement (if possible) the area required for drain field.
- 3) Sunshine Valley Developments Ltd. agree to the Ministry gazetting the proposed detour route as soon as can be arranged, and to allowing our contractor to construct and pave the detour upon the signing of this letter.
- 4) The Ministry will install no stopping signs along the detour route and will permit advertising of Sunshine Valley Developments Ltd. on the detour route. An entrance sign will be permitted where the old Highway 3 route meets the proposed detour.
- 5) Sunshine Valley Developments Ltd. will be allowed unimpeded access to the detour route when traffic is removed from the detour on or about July 31, 1981.
- 6) Prior to July 31, 1981 any work on the detour will have to be co-ordinated with our Regional Construction Engineer, every effort will be made to accommodate Sunshine Valley Developments Ltd.
- 7) On or about July 31, 1981 the Ministry of Transportation and Highways will remove the traffic from the detour and will de-gazette all Right-of-Ways as per our Right-of-Way Agreement and this Letter of Understanding. This de-gazetting and vesting should be complete by December 31, 1981.

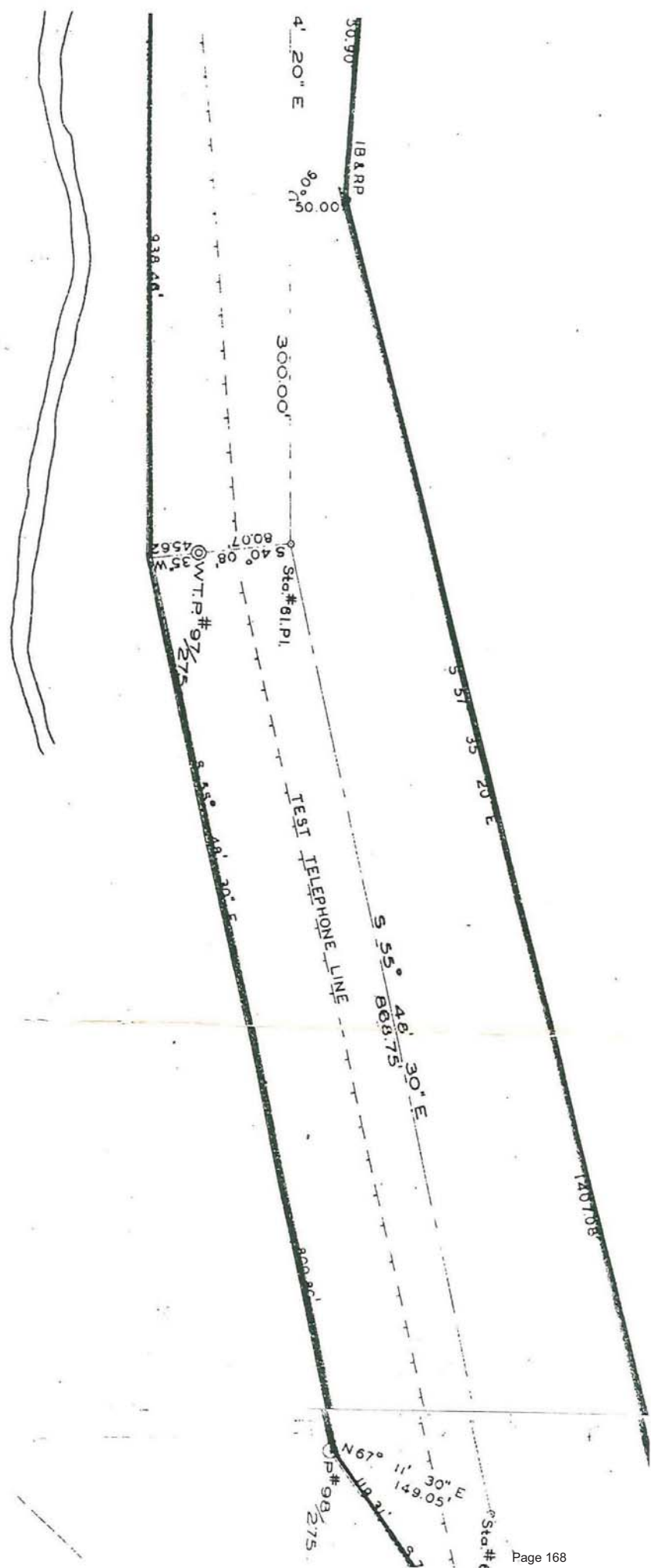


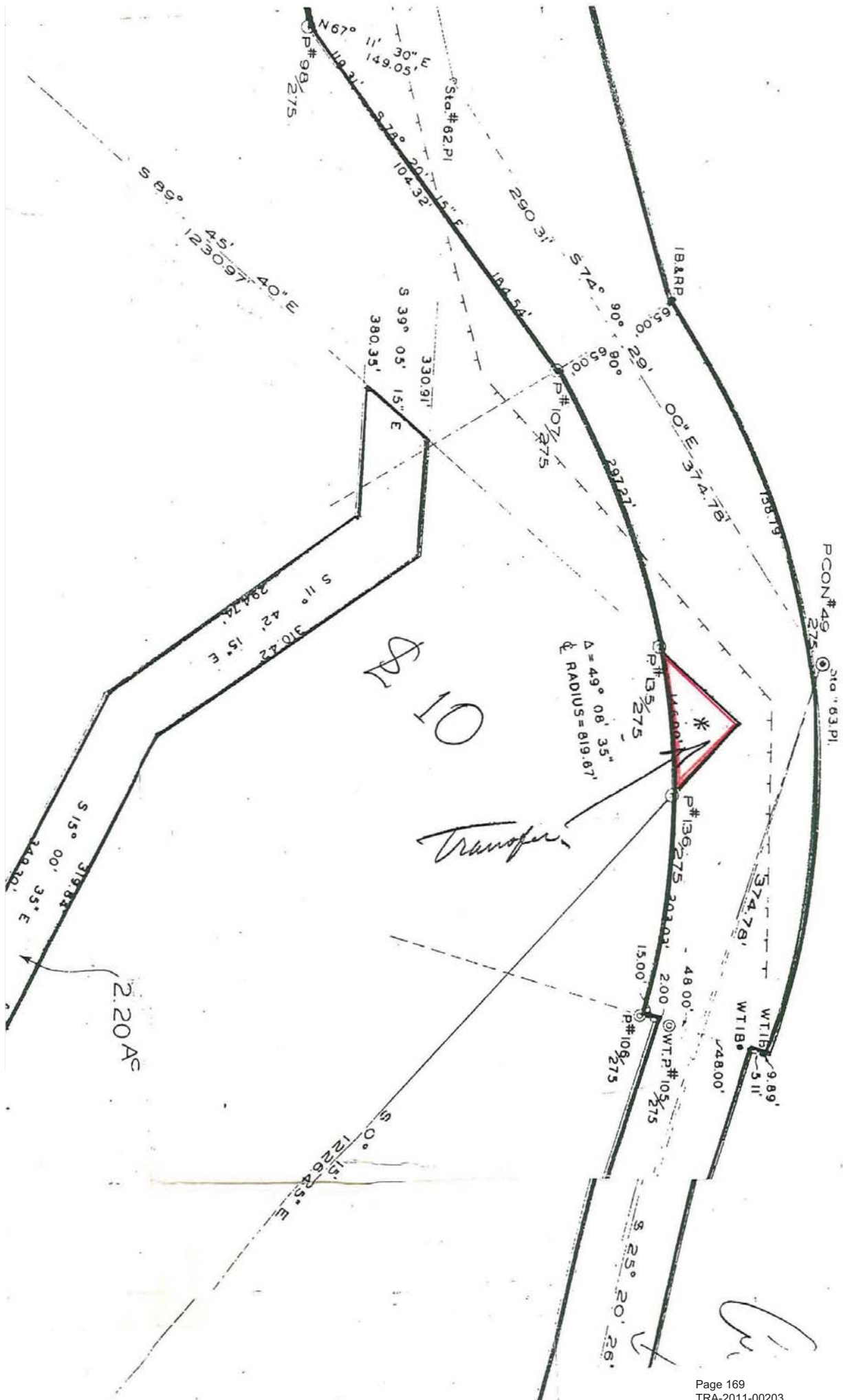
M.J. O'Connor  
Regional Highway Engineer  
Ministry of Transportation  
and Highways



D. Low  
President  
Sunshine Valley  
Developments Ltd.













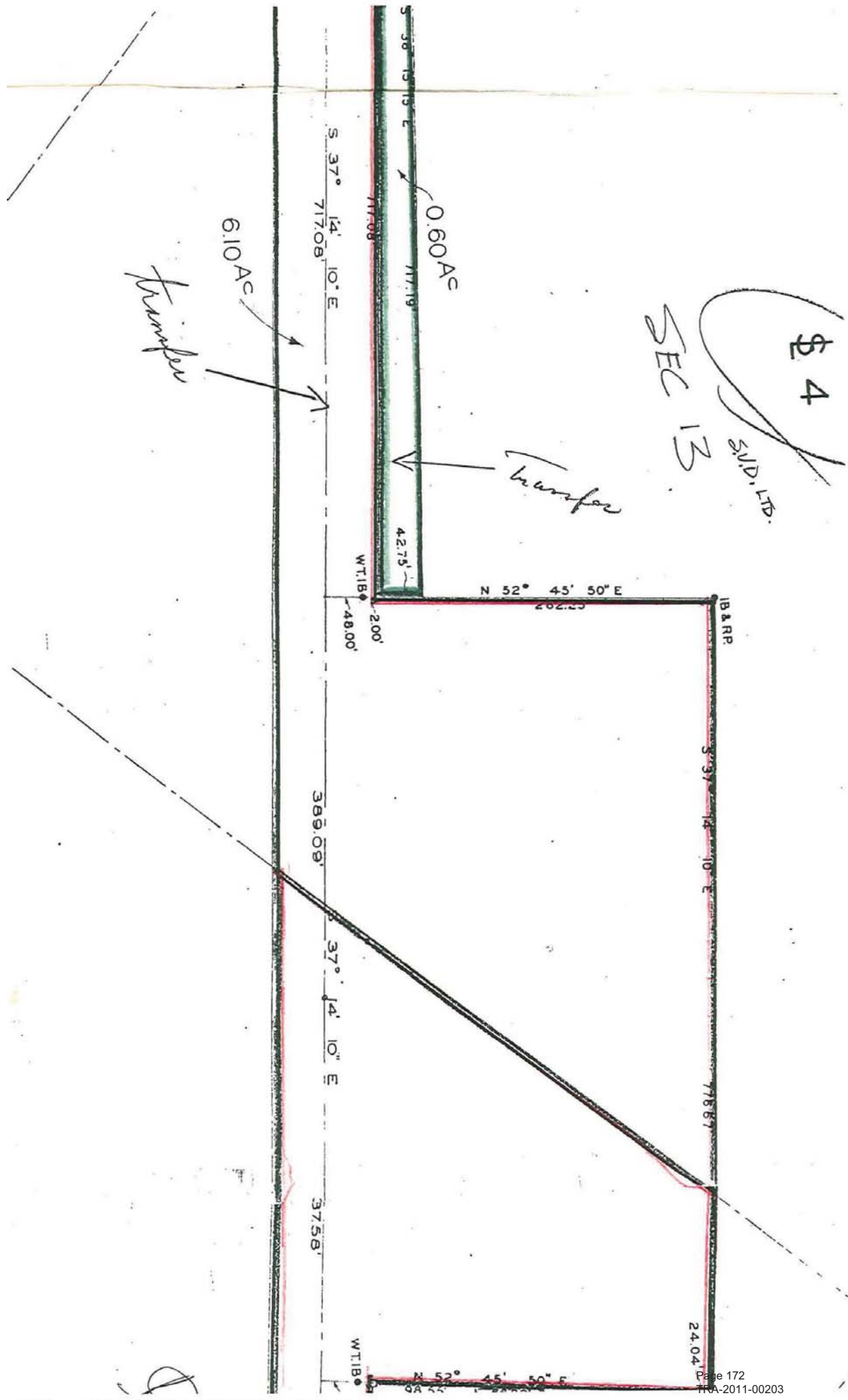
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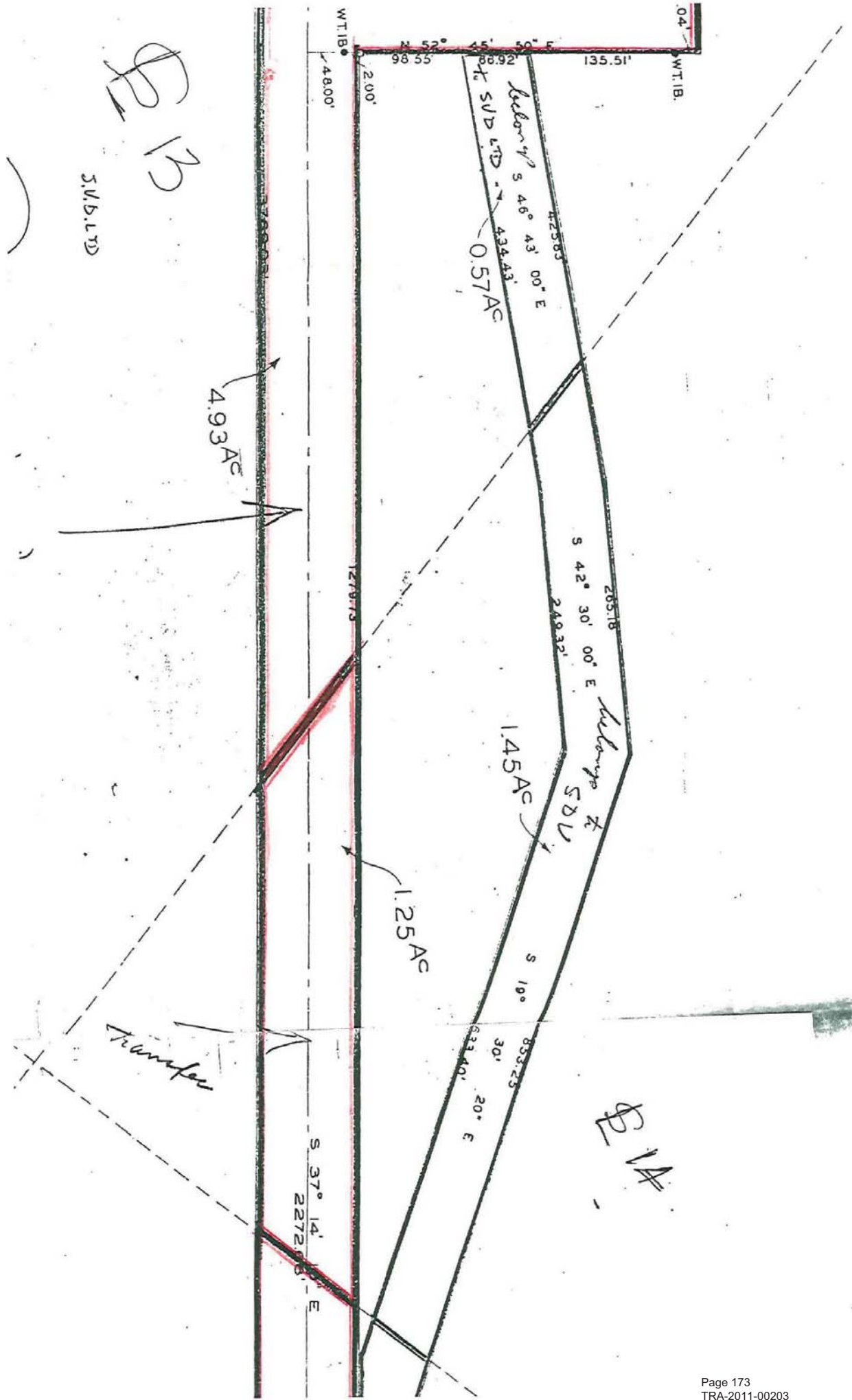
SEC 13

S.D. LTD.

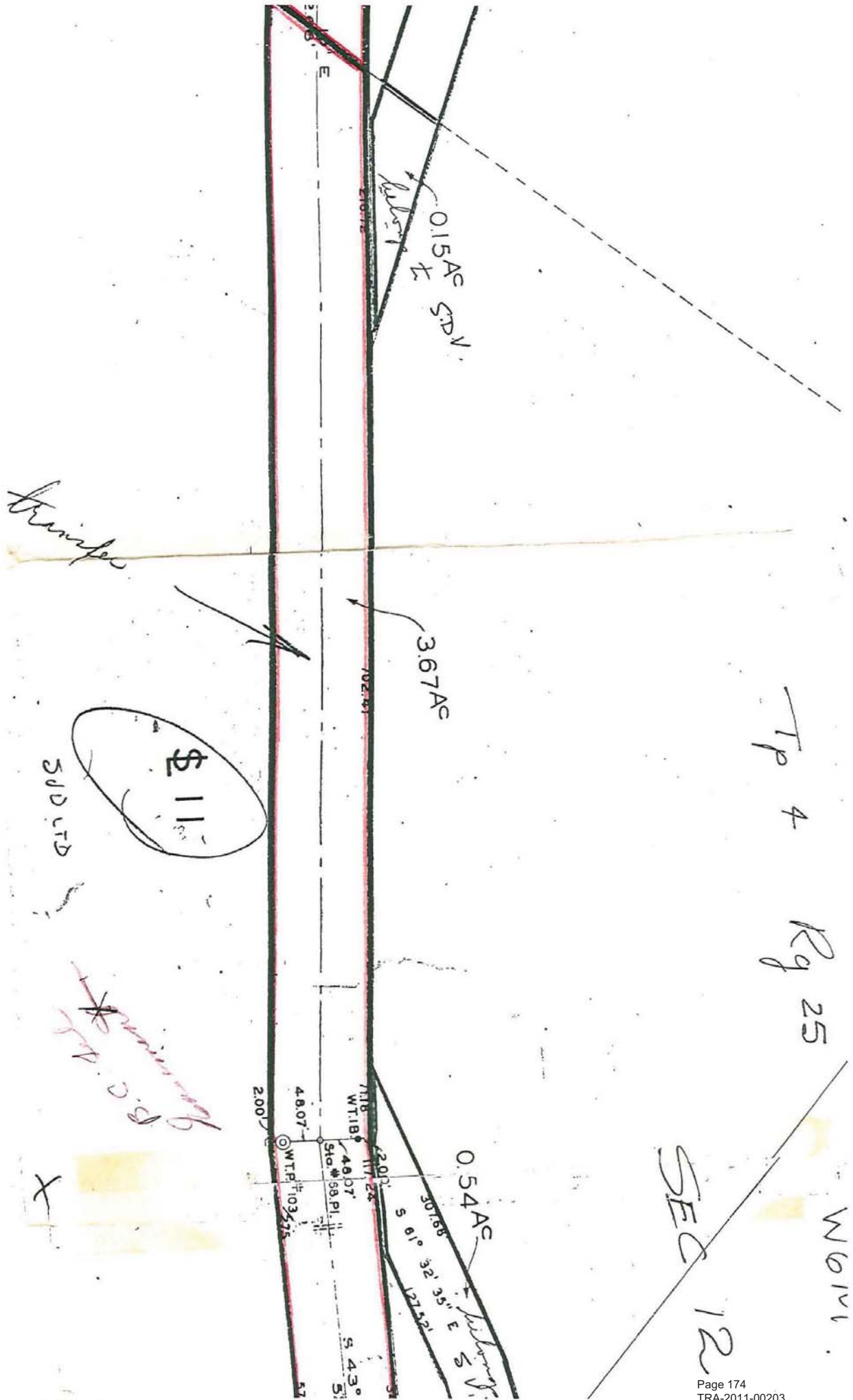
Transfer

6.10 AC  
Transfer





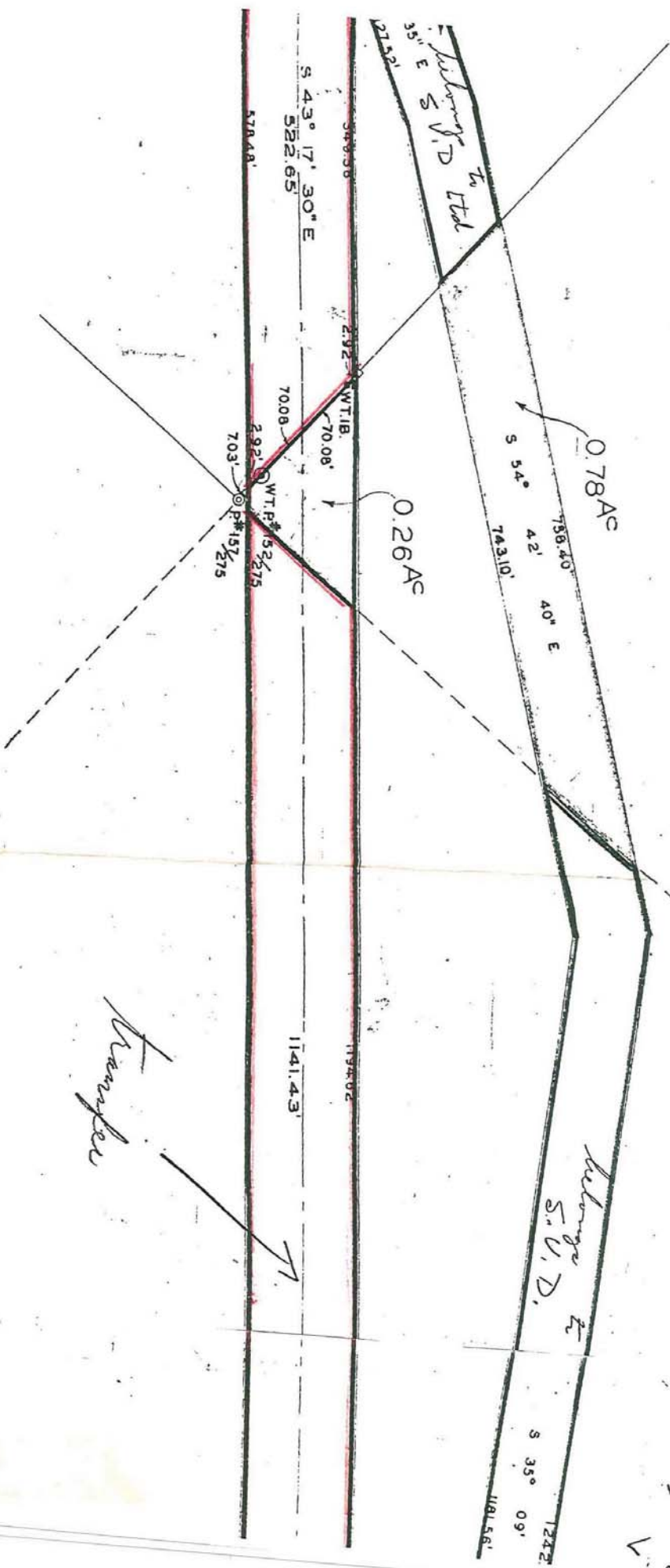




M

12

*Johnson*



S.D. LTD.

SE 1/4 SEC 12

①

1242.09  
09' 43" E  
181.56'

IBARP  
2000'

200'  
WTIB  
4800'

1414.42

1429.07

550.91

5.22 AC

S 48° 11' 35" E  
803.18

237.66' belong to S.D. LTD.

S 80° 00' 05" E  
344.93  
348.39'

859.30'

8.60 AC

243° 17' 30" E  
392.00'

45.70'

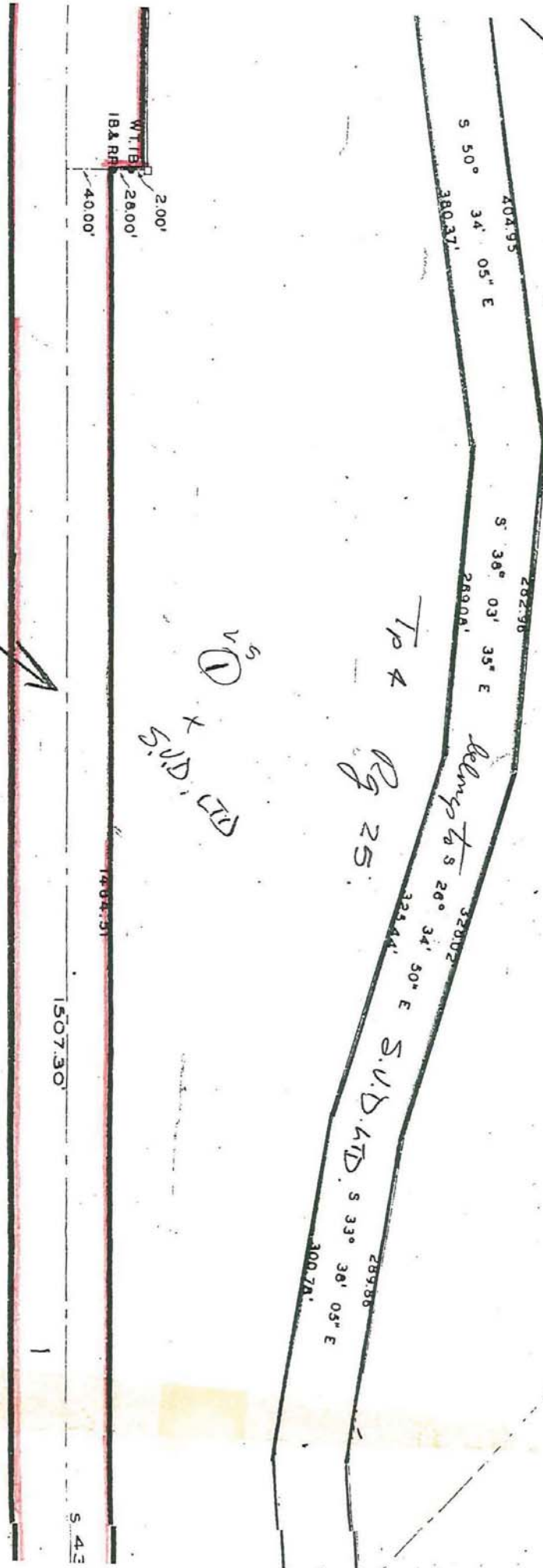
B.C. de  
Maurice

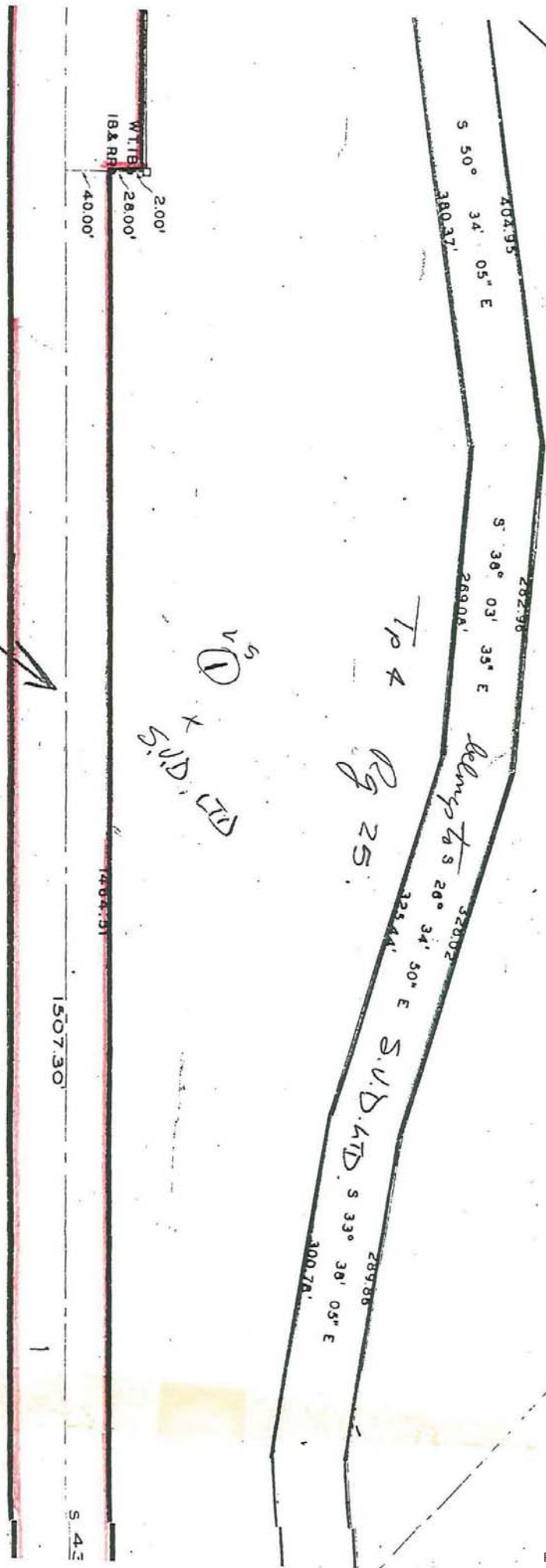
Transfer

B.C. de  
Maurice

Transfer







SEC 6

2.49 AC

Quinn

0.38 AC

S 50° 25' 10" E

S 50° 25' 10" E

\$ 13

NW 1/4, R 24, W 6M.  
OR @ PCON # 144/275

WT.I.B.

WT.I.B.

WT.I.B.

WT.I.B.

S 43° 17' 3"

S 43° 17' 3"

S 43° 17' 3"

S 43° 17' 30"

S 43° 17' 30"

WT. ROCK # 151/275

0.40 AC

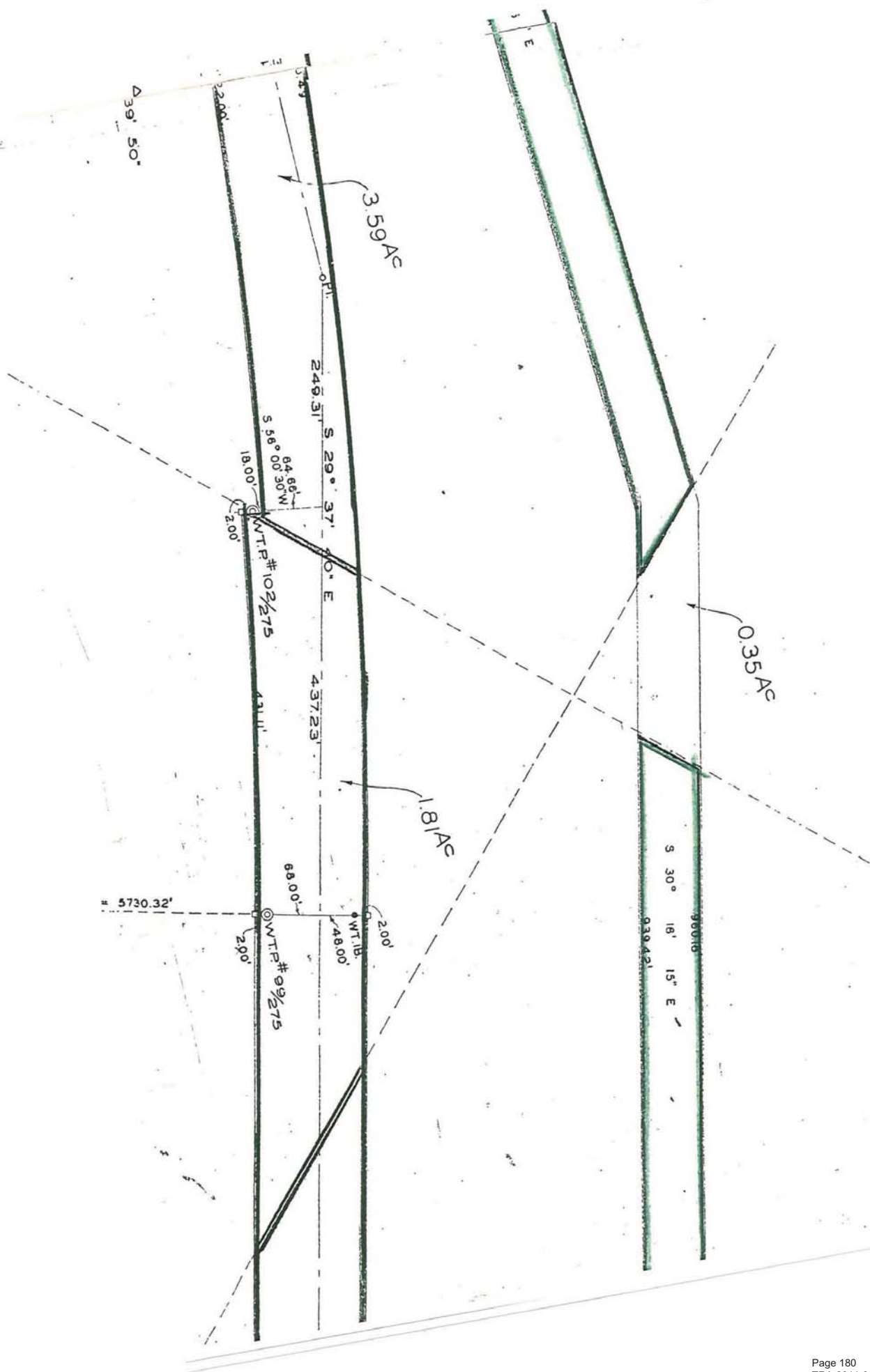
\$ 16

SEC 1

no plumb at point  
transfer the point  
10/6/20

transfer





SEC 6  
TD SEC 6

OBT  
10° FIR S 42° 30' W 12 LKS.  
ADDED NEW B.T.S.  
10° FIR S 49° E 24.7'  
8° FIR S 83° W 15.3'  
8° FIR N 46° E 13.1'

NW 1/4 7 S 6 T  
COR P# 139 / 275

2.63Ac

S 10° 22' 40" E

2.11

S 50° 55' E

S 50° 55' E

S 50° 32' 25" E

S 50° 32' 25" E

S 64° 09' 25"

0.37Ac

2.37Ac

SEC 6

Transfer

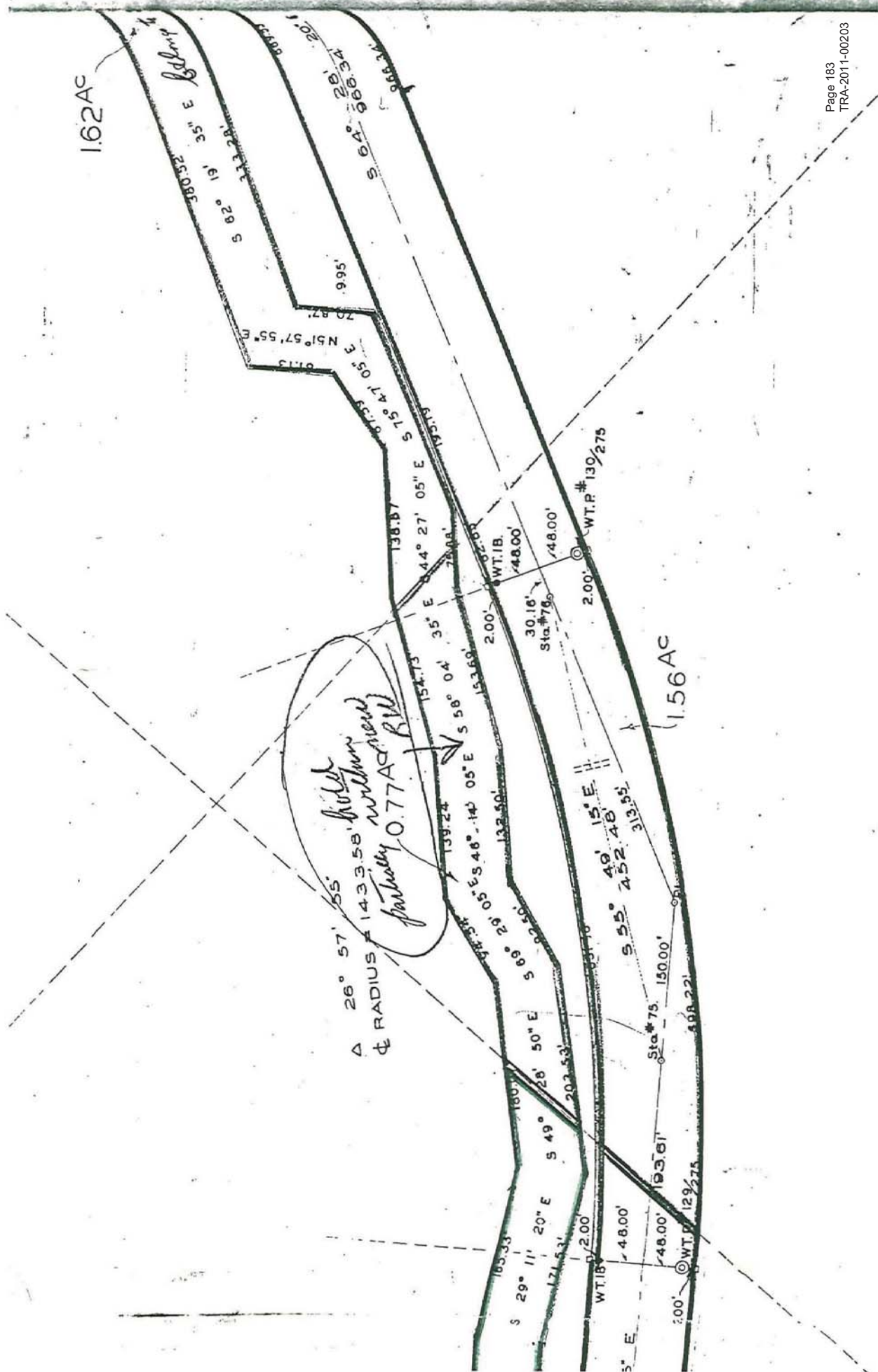
N 89° 44' 43" E

US = 716.55'

- 15' 55"

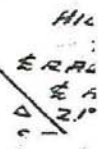








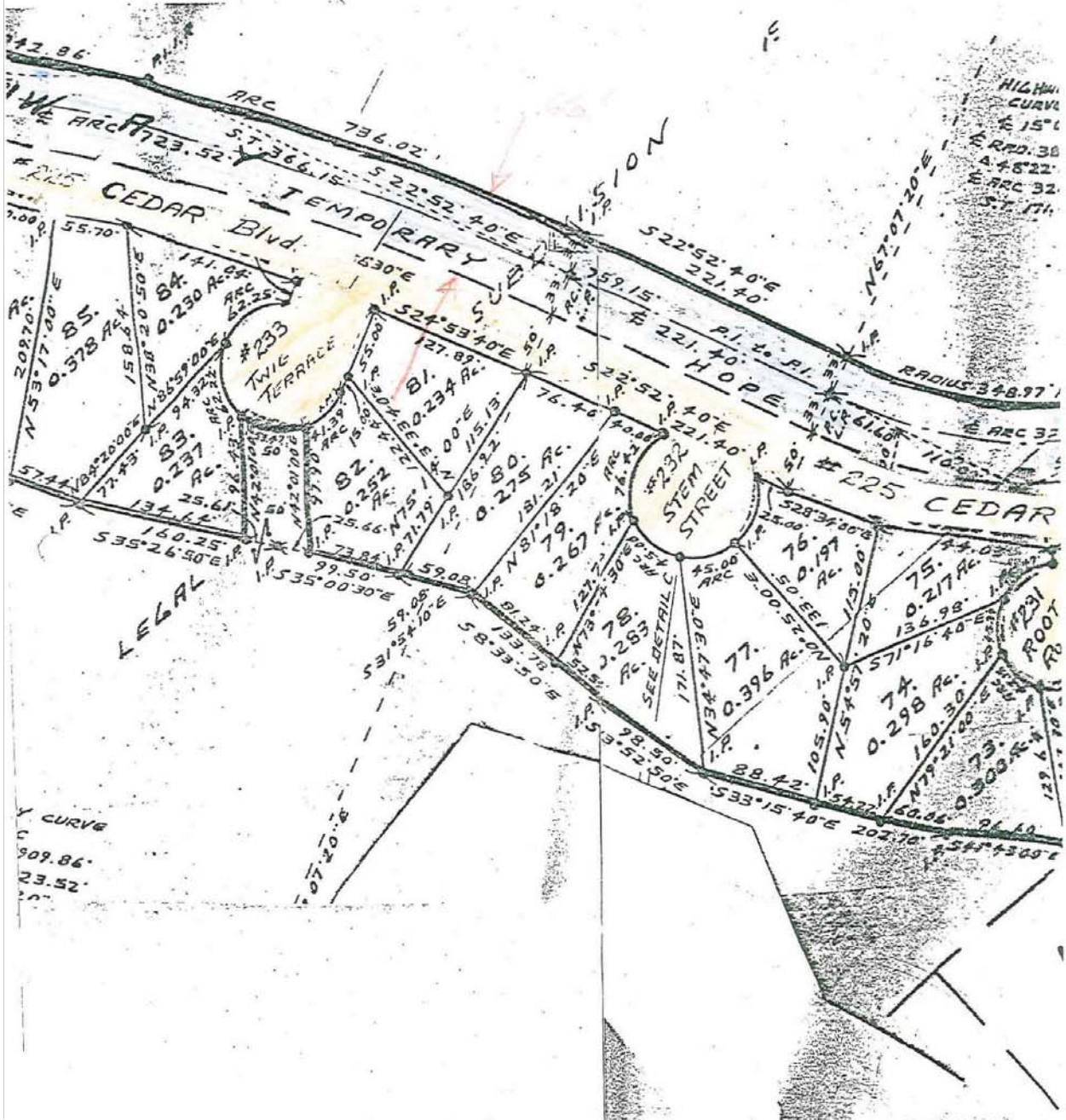
L.S. 9 SEC. 1  
 T.P. 4, R. 25  
 R/W = 1.23 Ac.





AN 22237

16' extra R/W  
Should be negotiated  
no problem for R/W



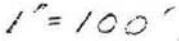










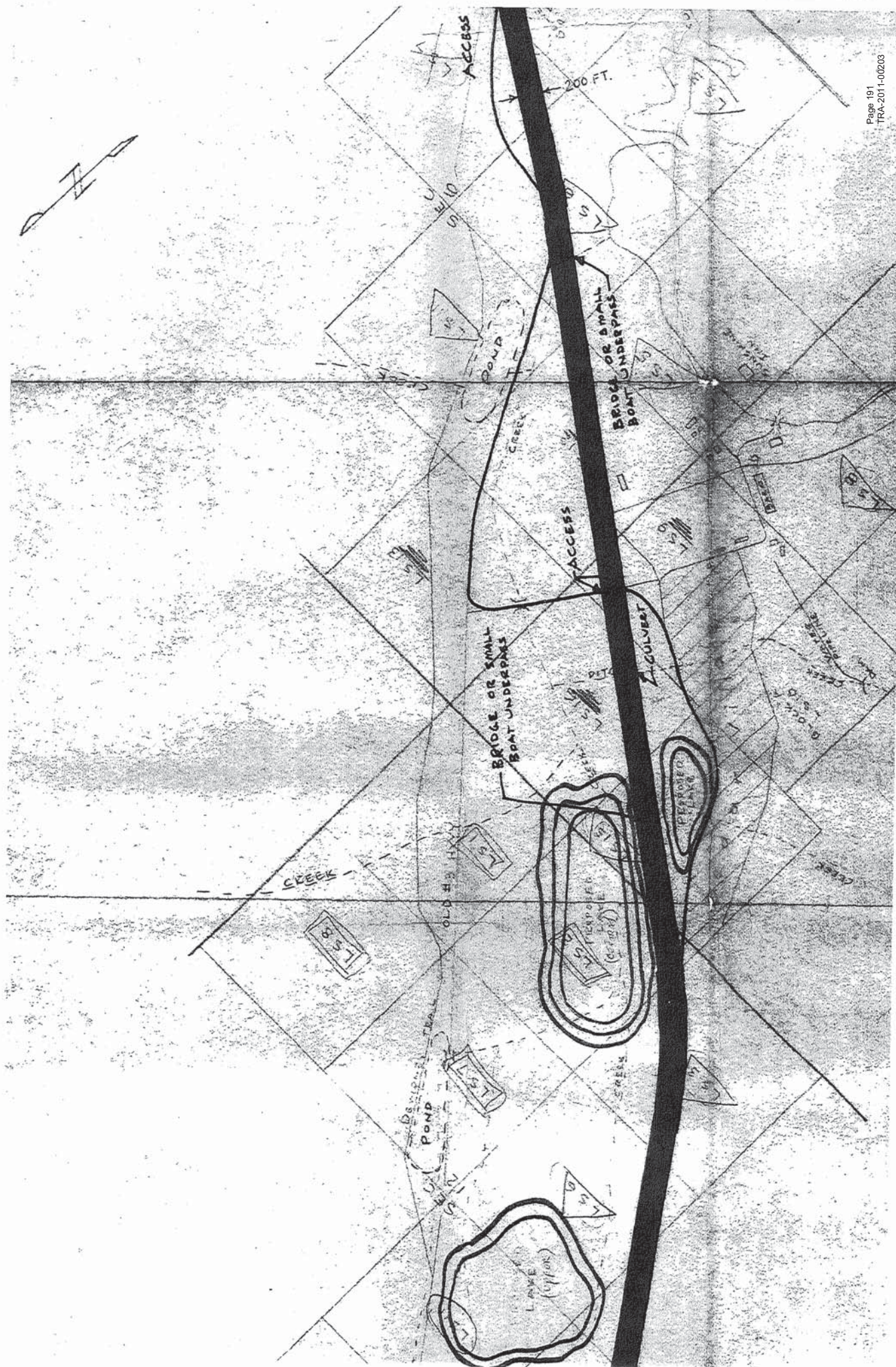


9











Forming part of agreement with

Name : SUNSHINE VALLEY DEVELOPMENTS LTD.

Description: T. C. H. NOCOLUM CREEK - HOPE SLIDE

It is agreed and understood that the Ministry of Highways intends to close and transfer those portions of old highway R/W Plan H 145 which cross private lands being the property of the owners herein and will also close and transfer those portions of existing travelled R/W not required for construction purposes after the construction has been completed. Adjustments between areas required and those to be transferred have been made and deducted from the compensation payable.

The aforementioned closures and transfers shall be subject to the approval of the Lieutenant-Governor-In-Council.

*JMK.* The Ministry of Highways agrees to expedite construction of this project through claimant's property and complete same by 1980. *78/8/15* *agreed* *Disposal*

The owners herein hereby agree to provide or allow for the necessary easements of the B. C. Hydro telephone facilities along the old or existing R/W's as required.

There shall be no access onto or crossings over the controlled access highway except at the designated intersections.

No further commitments.

*Pat Miller*

*Disposal*